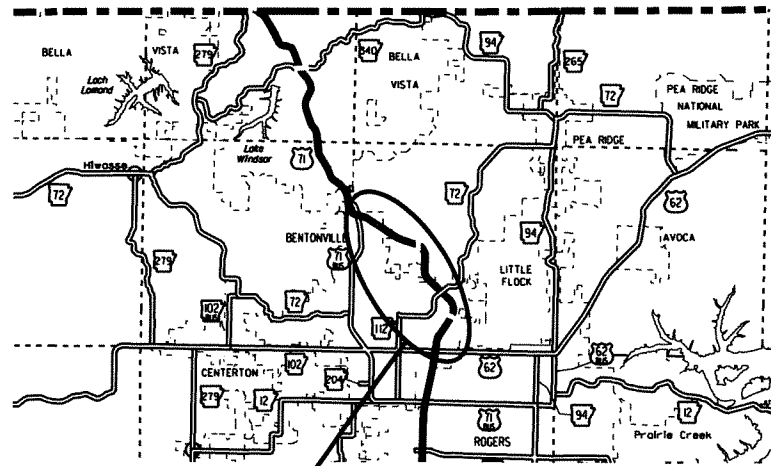


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090369	1	34	

2 HWY. 62/102-NORTH (CABLE MEDIAN BARRIER) (S)



PROJECT LOCATION

VICINITY MAP

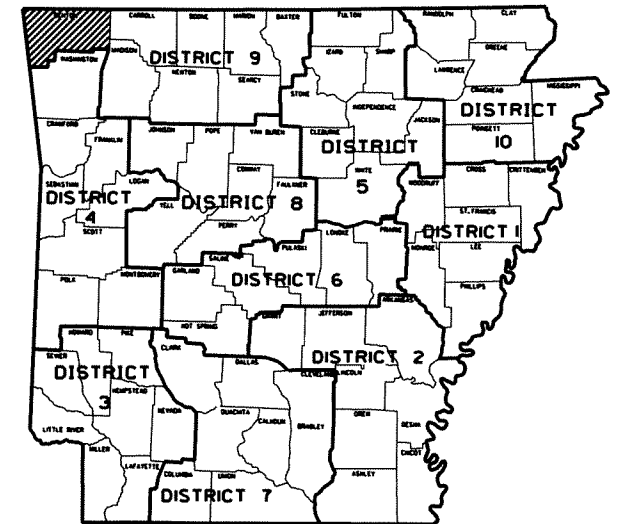
"A FULLY CONTROLLED ACCESS FACILITY"
 ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
 CONSTRUCTION PLANS FOR STATE HIGHWAY

HWY. 62/102-NORTH
 (CABLE MEDIAN BARRIER) (S)

BENTON COUNTY
 ROUTE 71 SECTION 19

JOB 090369

FED. AID PROJ. HSIP-9036(14)



ARK. HWY. DIST. NO. 9

DESIGN TRAFFIC DATA

DESIGN YEAR	2013
2013 ADT	36,000
2033 ADT	51,000
2033 DHV	5610
DIRECTIONAL DISTRIBUTION	60%
TRUCKS	10%
DESIGN SPEED	70 MPH

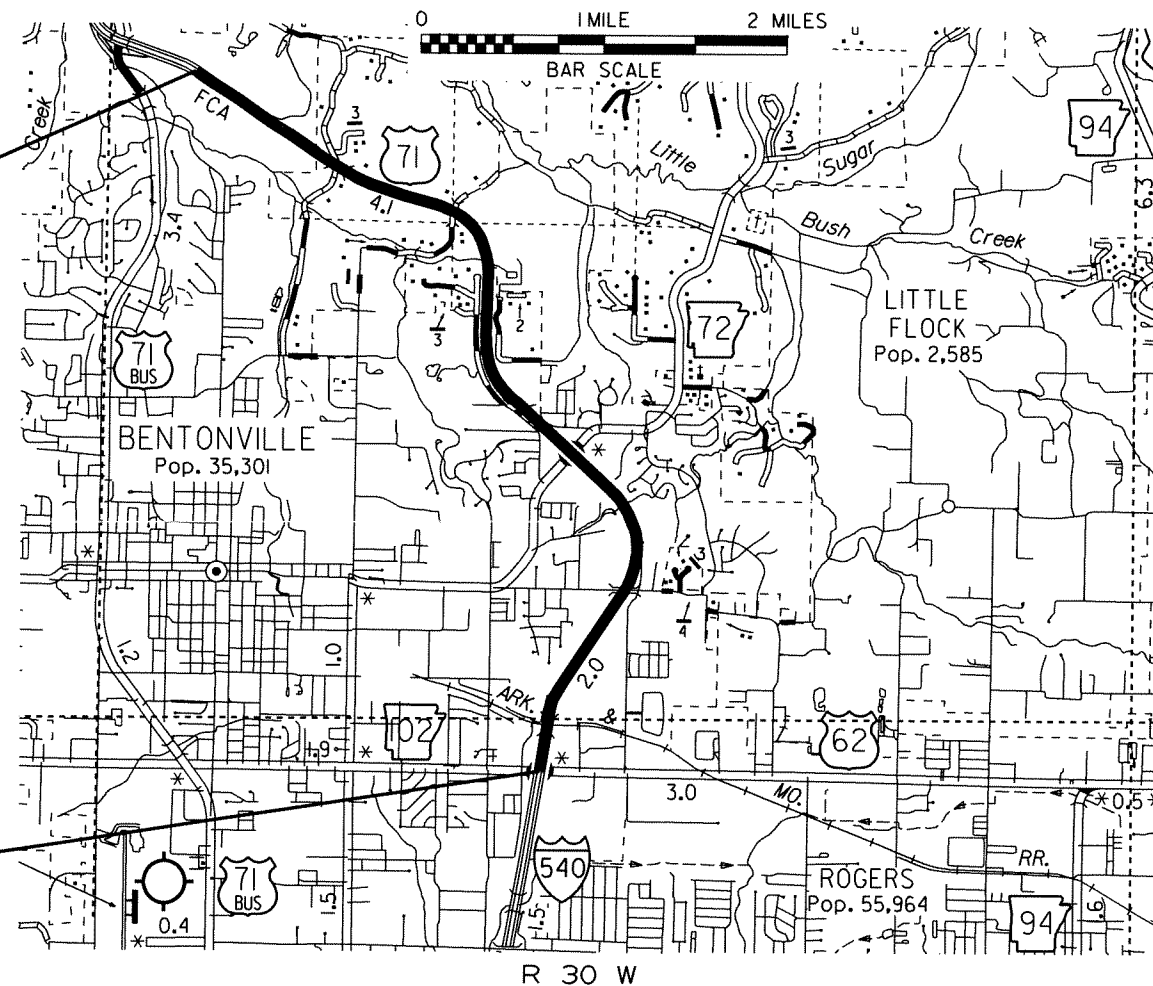
STA. 1297+75
 END JOB 090369
 LOG MILE 5.03

EXCEPTIONS

STA. 1043+50 - STA 1045+65 [215']
 STA. 1256+15 - STA 1258+15 [200']

TOTAL LENGTH OF EXCEPTIONS = 415'

STA. 1032+00
 BEGIN JOB 090369
 LOG MILE 0.00



T 20 N



T 19 N

R 30 W

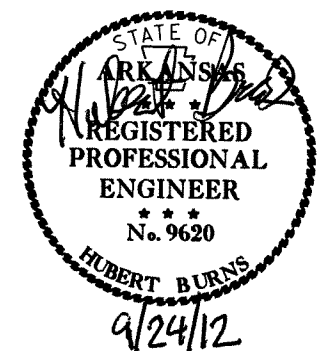
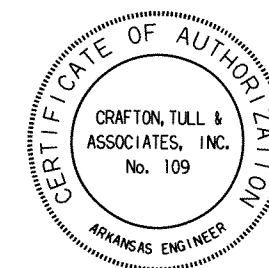
BEGINNING OF PROJECT
 LAT. = N 36°21' 24"
 LONG. = W 94°10' 41"

MID-POINT OF PROJECT
 LAT. = N 36°23' 11"
 LONG. = W 94°10' 55"

END OF PROJECT
 LAT. = N 36°24' 39"
 LONG. = W 94°12' 35"

GROSS LENGTH OF PROJECT	26575.00	FEET	OR	5.033	MILES
NET " " ROADWAY	26160.00	"	"	4.955	"
NET " " BRIDGES	0.00	"	"	0.000	"
NET " " PROJECT	26160.00	"	"	4.955	"

P.E. 090369
 NON-PART.



9/24/12

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
11-06-12				6	ARK.			
				JOB NO.	090369	2	34	

2 INDEX OF SHEETS, GOV. SPECS. & GEN. NOTES



INDEX OF SHEETS

SHEET NO.	TITLE	DRWG. NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES		
3	TYPICAL SECTIONS OF IMPROVEMENT		
4-5	SPECIAL DETAILS		
6-10	TEMPORARY EROSION CONTROL DETAILS		
11-13	MAINTENANCE OF TRAFFIC		
14-16	QUANTITY SHEETS		
17	SUMMARY OF QUANTITIES AND REVISIONS		
18-22	PLAN SHEETS		
23	CONCRETE DITCH PAVING.....	CDP-1.....	11-17-10
24	GUARD RAIL DETAILS.....	GR-8.....	7-14-10
25	GUARD RAIL DETAILS.....	GR-8A.....	7-14-10
26	GUARD RAIL DETAILS.....	GR-9.....	4-17-08
27	GUARD RAIL DETAILS.....	GR-9A.....	4-17-08
28	GUARD RAIL DETAILS.....	GRT-1.....	7-14-10
29	CONCRETE PIPE CULVERT FILL HEIGHTS & BEDDING.....	PCC-1.....	12-15-11
30	METAL PIPE CULVERT FILL HEIGHTS & BEDDING.....	PCM-1.....	12-15-11
31	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION.....	TC-1.....	12-15-11
32	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION.....	TC-2.....	3-11-10
33	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION.....	TC-3.....	10-15-09
34	TEMPORARY EROSION CONTROL DEVICES.....	TEC-1.....	12-15-11

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

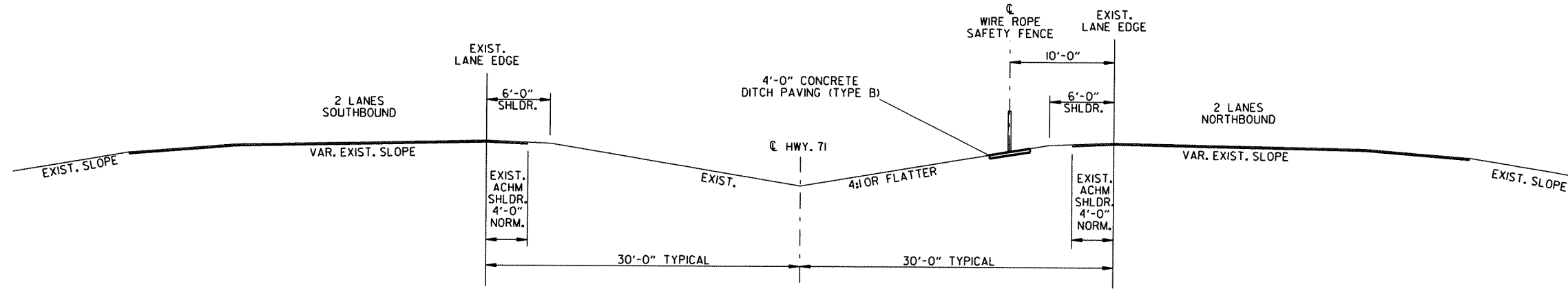
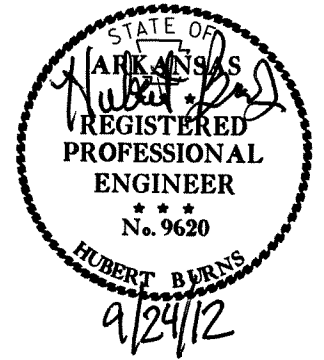
NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-2	MANUAL FOR ASSESSING SAFETY HARDWARE (MASH)
102-1	BIDDING REQUIREMENTS AND CONDITIONS
103-1	DETERMINATION OF DBE PARTICIPATION
105-1	CONSTRUCTION CONTROL MARKINGS
105-2	EQUIPMENT AND MATERIAL STORAGE ON BRIDGE STRUCTURES
105-3	CONTROL OF WORK
107-1	WORKER VISIBILITY
108-1	LIQUIDATED DAMAGES
303-1	AGGREGATE BASE COURSE
404-1	PRODUCTION VERIFICATION OF ASPHALT CONCRETE HOT MIX
404-2	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
409-1	MINERAL AGGREGATES
410-3	DENSITY TESTING FOR ACHM LEVELING COURSES AND BOND BREAKERS
600-1	WATER FOR VEGETATION
603-1	MAINTENANCE OF TRAFFIC
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-2	INSPECTION OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 090369	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 090369	CONCRETE DITCH PAVING
JOB 090369	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 090369	INTERNET BIDDING
JOB 090369	MAINTENANCE OF TRAFFIC
JOB 090369	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIERS
JOB 090369	SEQUENCE OF CONSTRUCTION
JOB 090369	SITE USE (A + C METHOD)
JOB 090369	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 090369	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 090369	UTILITY ADJUSTMENTS
JOB 090369	WARM MIX ASPHALT
JOB 090369	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS
JOB 090369	WIRE ROPE SAFETY FENCE PARTS
JOB 090369	WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS
JOB 090369	WRSF TRAINING WORKSHOP

GENERAL NOTES

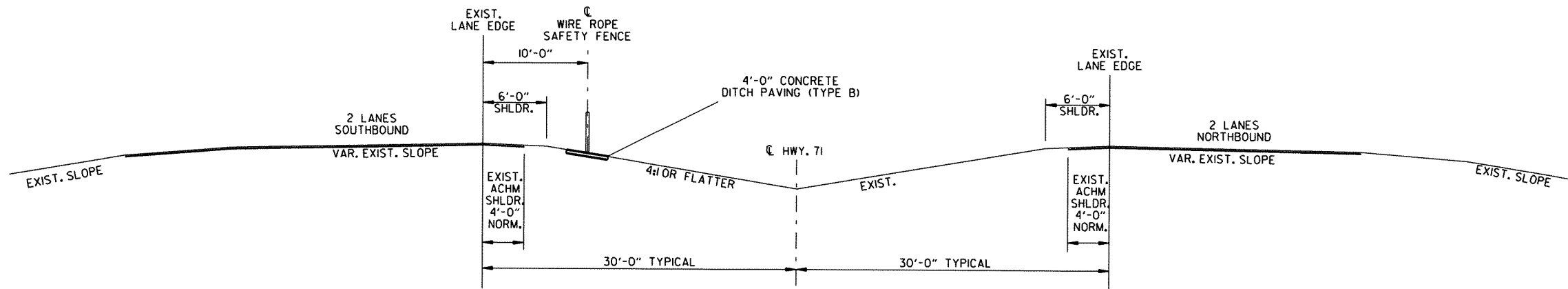
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- ANY REQUIRED EROSION CONTROL MEASURES FROM WASTING MATERIAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090369		3	34

2 TYPICAL SECTIONS OF IMPROVEMENT



TYPICAL SECTION OF IMPROVEMENT
WIRE ROPE SAFETY FENCE ON NORTHBOUND LANES FORESLOPE

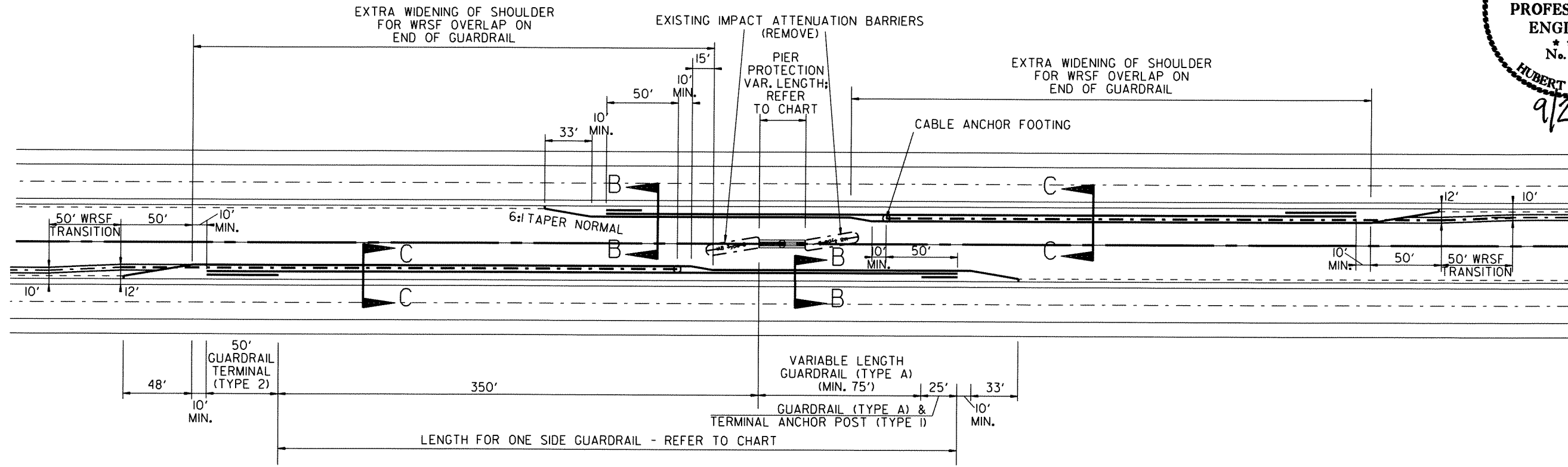
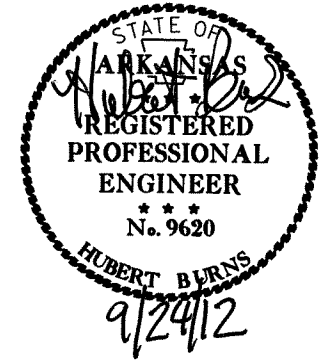


TYPICAL SECTION OF IMPROVEMENT
WIRE ROPE SAFETY FENCE ON SOUTHBOUND LANES FORESLOPE

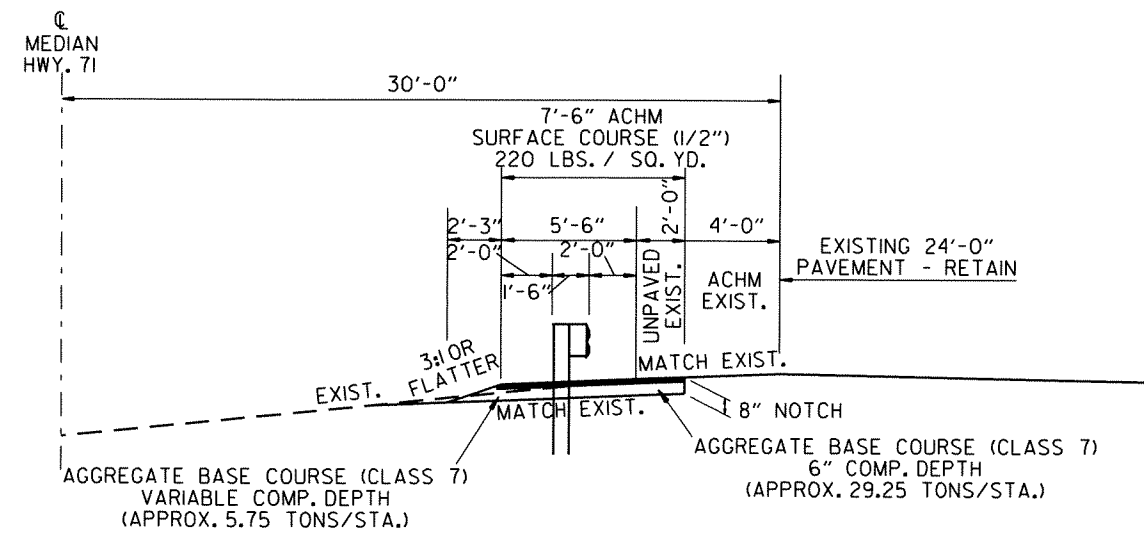
STATION	CROSSING ROAD NAME	BRIDGE NUMBER	PIER PROTECTION LENGTH	LENGTH FOR ONE SIDE GUARDRAIL
1089+71	BATTLEFLD. BLVD.	5981	21.5'	450'
1137+25	HWY. 72	5982	33'	475'
1220+20	N.E. "J" ST.	5983	21.5'	450'

• GUARDRAIL LENGTH IS ROUNDED UP BASED ON GUARDRAIL STOCK LENGTH OF 25.0'

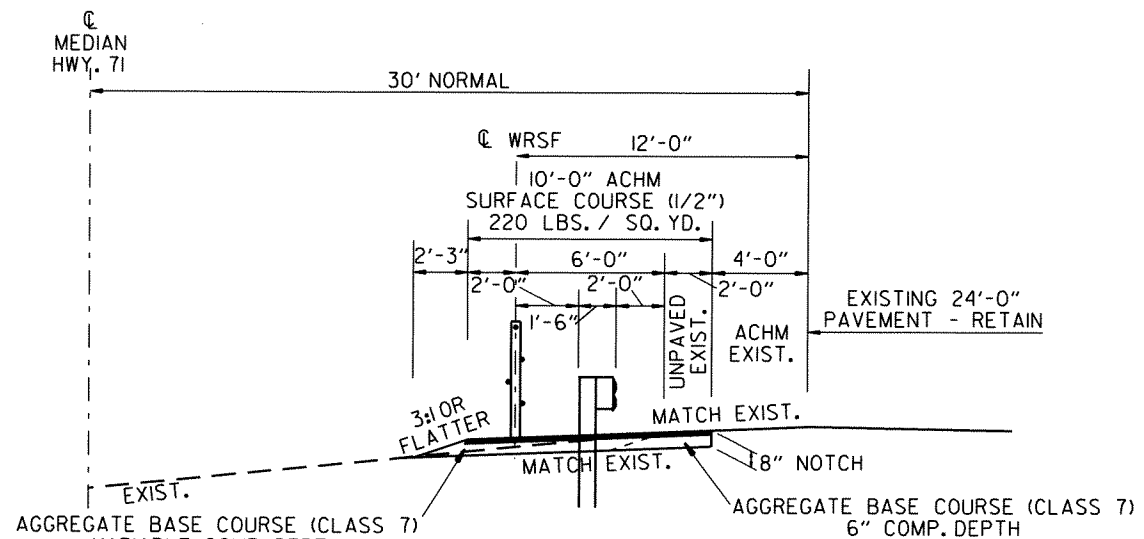
2 SPECIAL DETAILS



GUARDRAIL INSTALLATION IN 60' MEDIAN AT OVERPASS



SECTION B-B

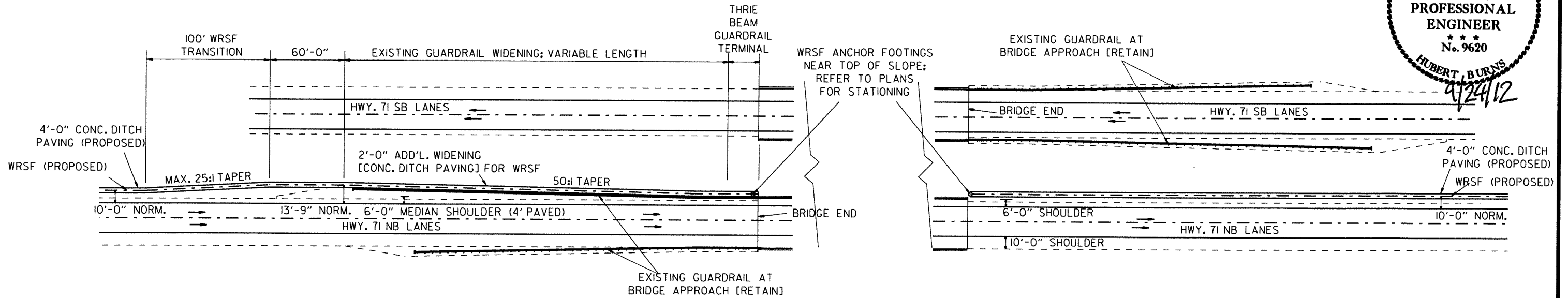
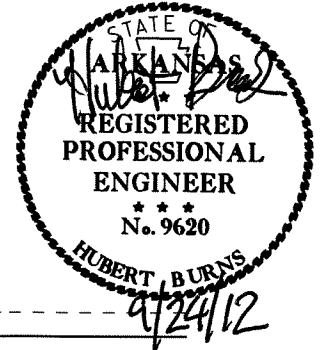


SECTION C-C

SPECIAL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.	090369	5	34

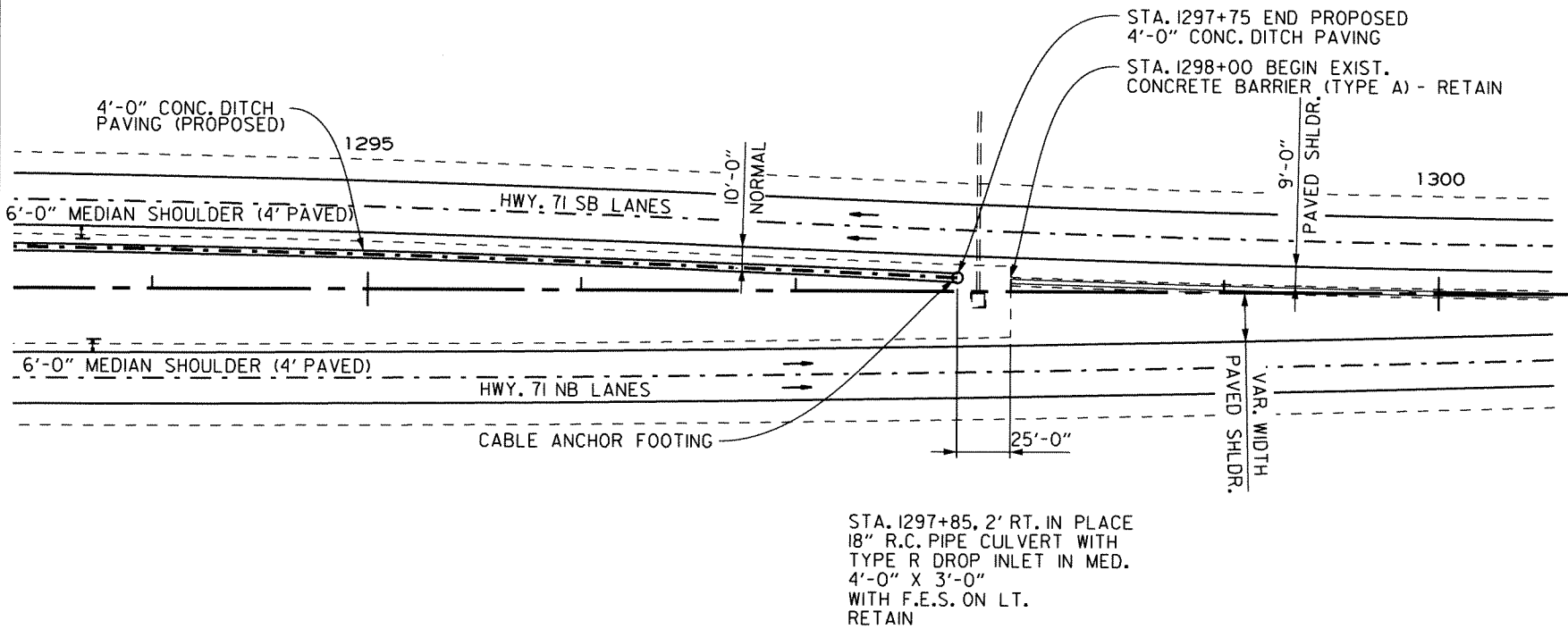
2 SPECIAL DETAILS



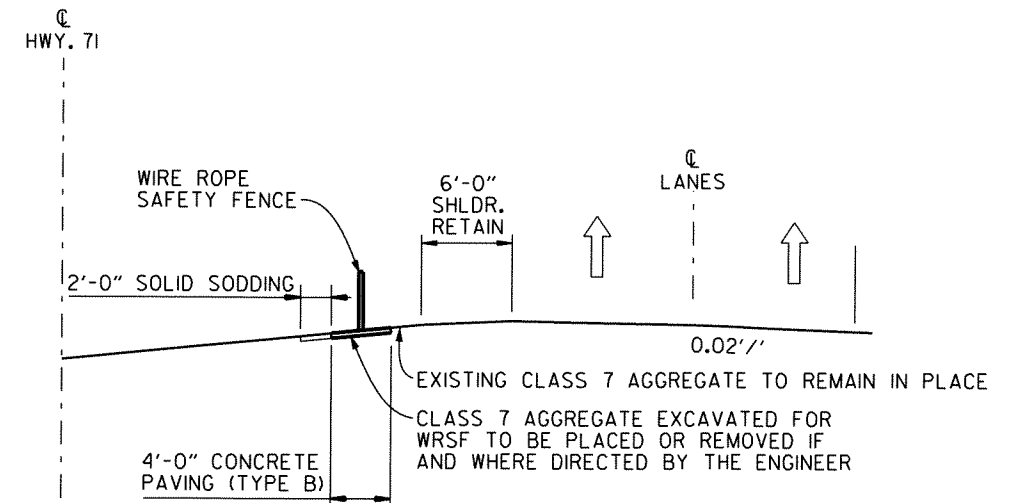
WRSF AND EXIST. GUARDRAIL ON SAME SIDE OF MEDIAN

WRSF AND EXIST. GUARDRAIL ON OPPOSITE SIDES OF MEDIAN

DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING BRIDGE ENDS

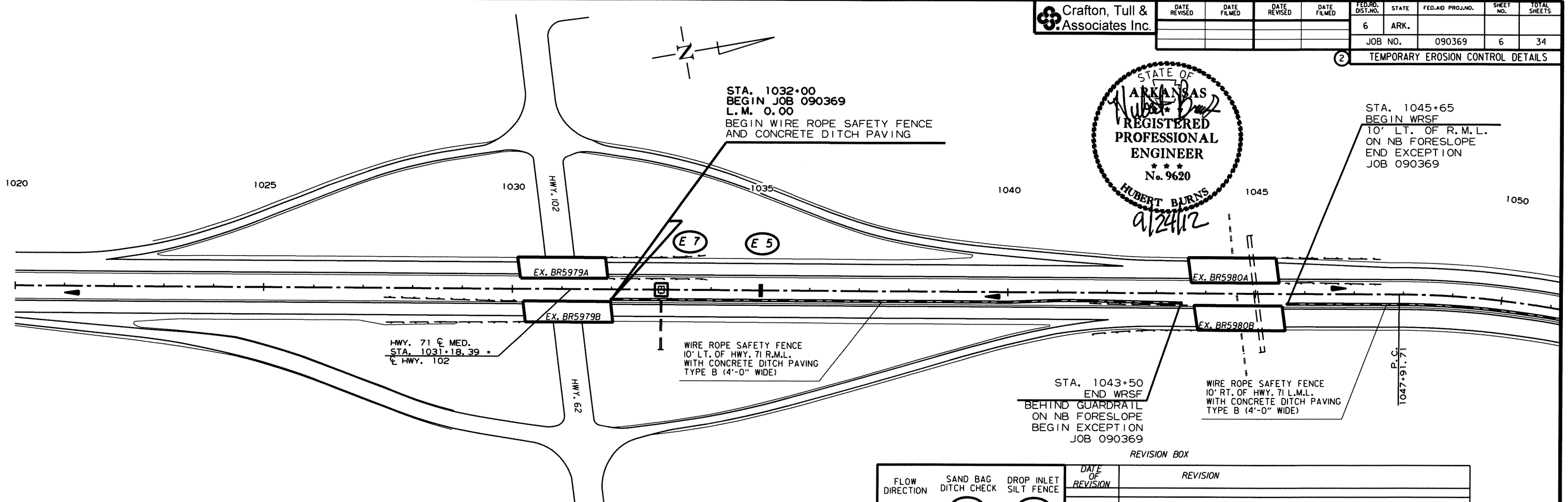
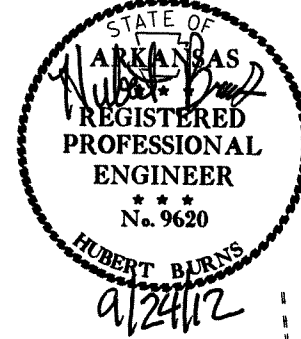


DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING MEDIAN BARRIER END

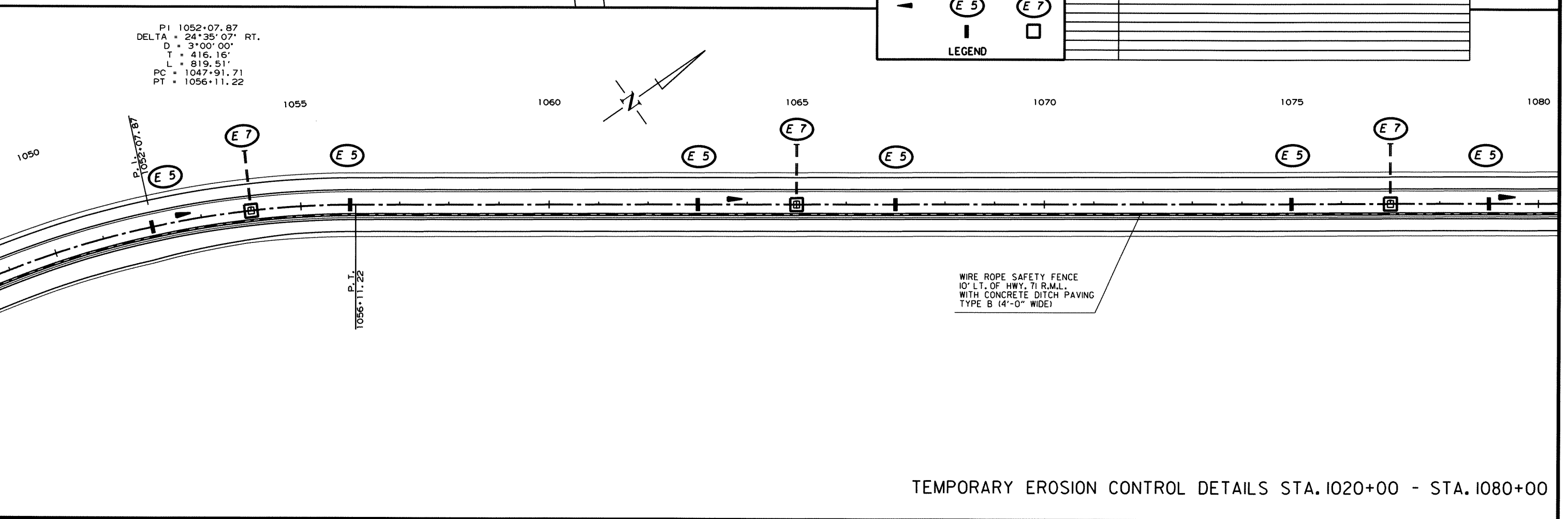


SOLID SODDING DETAIL

SPECIAL DETAILS

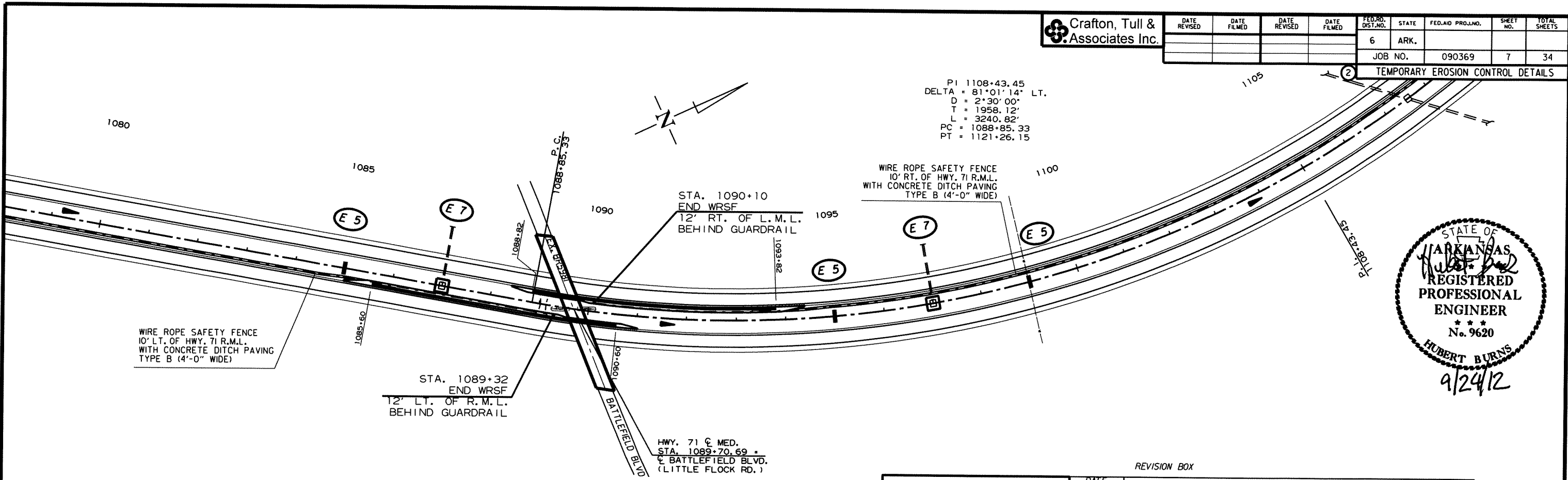


PI 1052+07.87
 DELTA = 24°35'07" RT.
 D = 3°00'00"
 T = 416.16'
 L = 819.51'
 PC = 1047+91.71
 PT = 1056+11.22

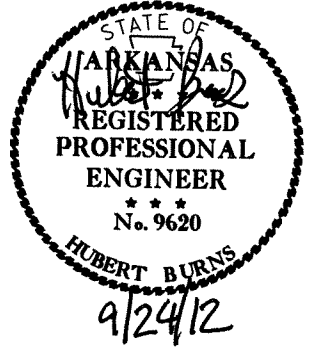


TEMPORARY EROSION CONTROL DETAILS STA. 1020+00 - STA. 1080+00

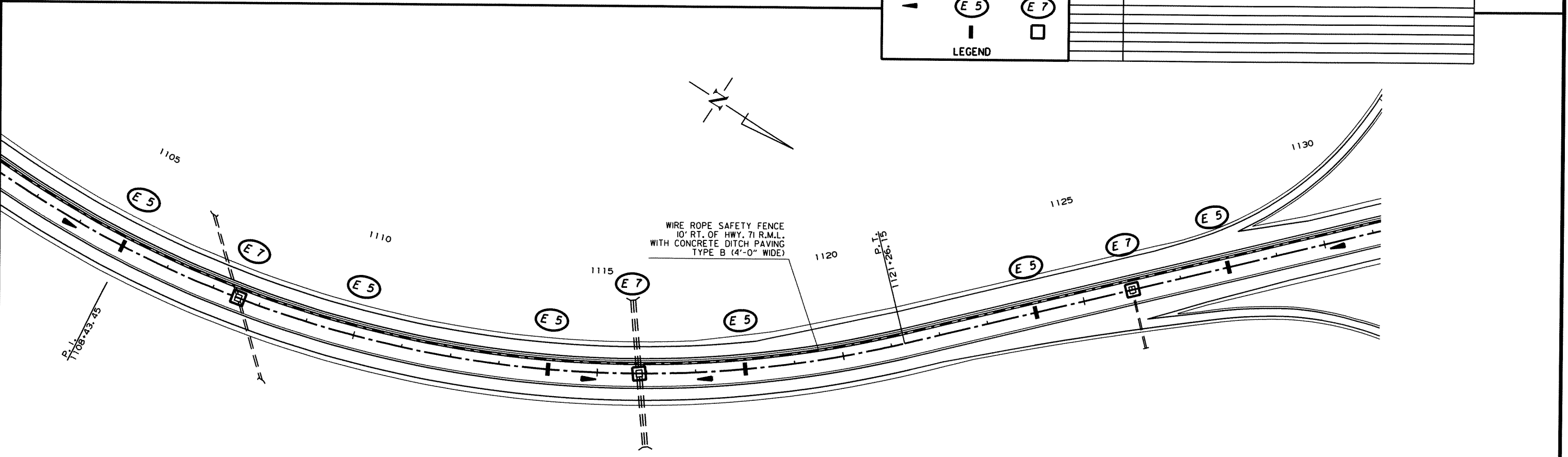
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		090369	7	34
TEMPORARY EROSION CONTROL DETAILS								



PI 1108+43.45
 DELTA = 81°01'14" LT.
 D = 2°30'00"
 T = 1958.12'
 L = 3240.82'
 PC = 1088+85.33
 PT = 1121+26.15



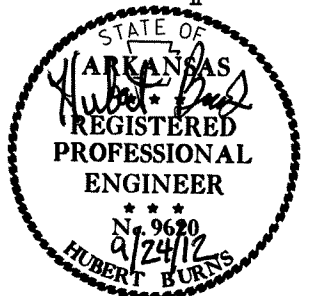
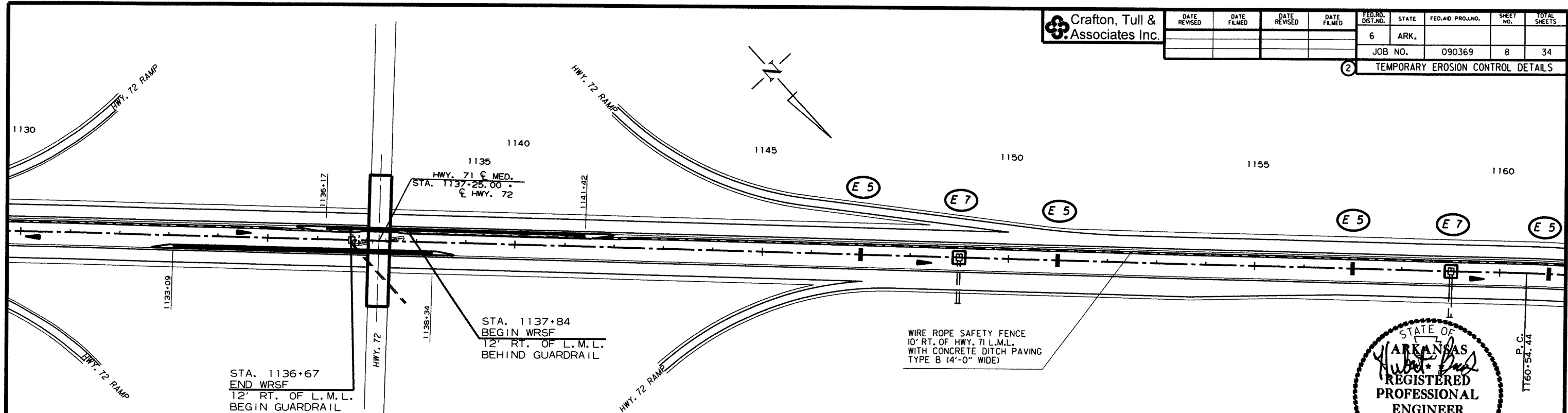
LEGEND			REVISION BOX	
FLOW DIRECTION	SAND BAG DITCH CHECK	DROP INLET SILT FENCE	DATE OF REVISION	REVISION
←	(E 5)	(E 7)		
		□		



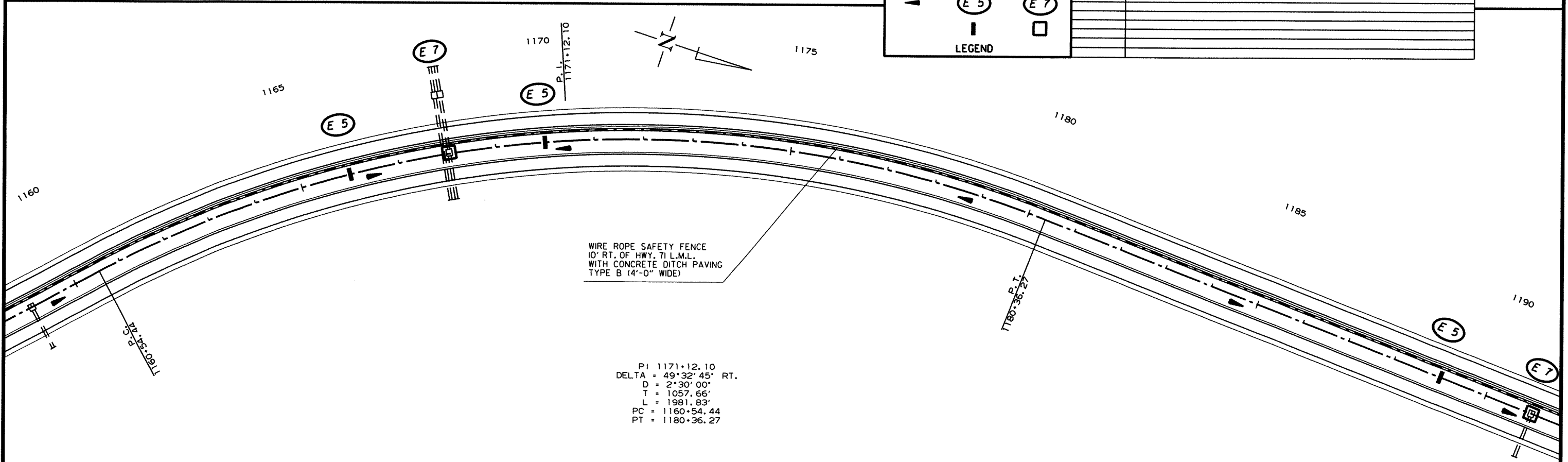
TEMPORARY EROSION CONTROL DETAILS STA. 1080+00 - STA. 1130+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 090369							8	34

2 TEMPORARY EROSION CONTROL DETAILS



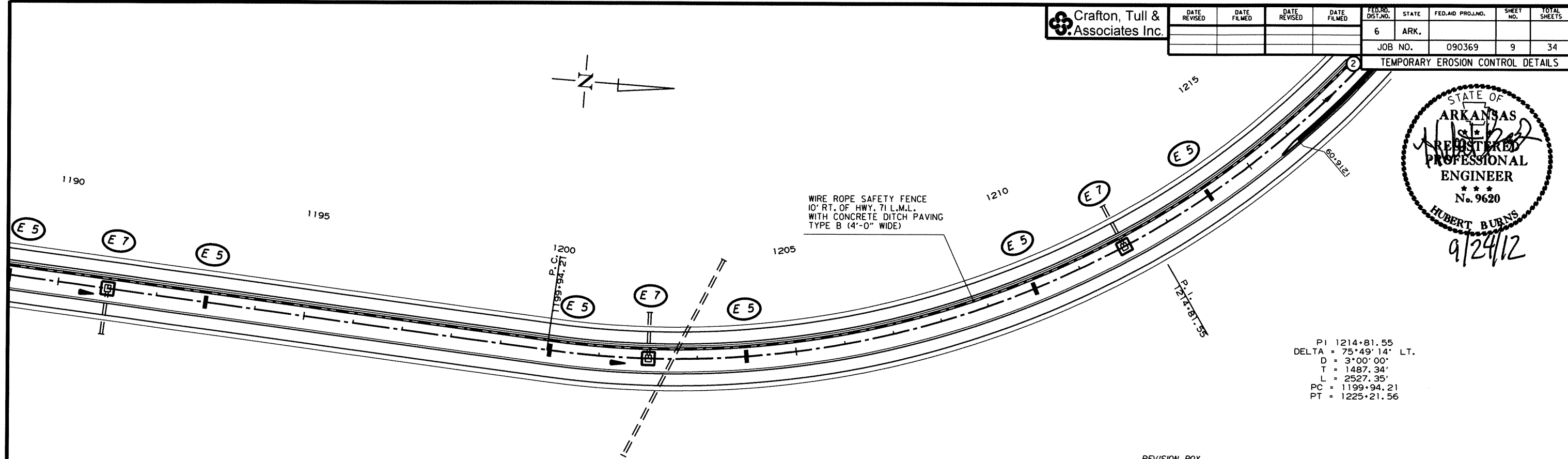
FLOW DIRECTION	SAND BAG DITCH CHECK	DROP INLET SILT FENCE	DATE OF REVISION	REVISION
←	E 5	E 7		
	I	□		
LEGEND				



TEMPORARY EROSION CONTROL DETAILS STA. 1130+00 - STA. 1190+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 090369							9	34

TEMPORARY EROSION CONTROL DETAILS



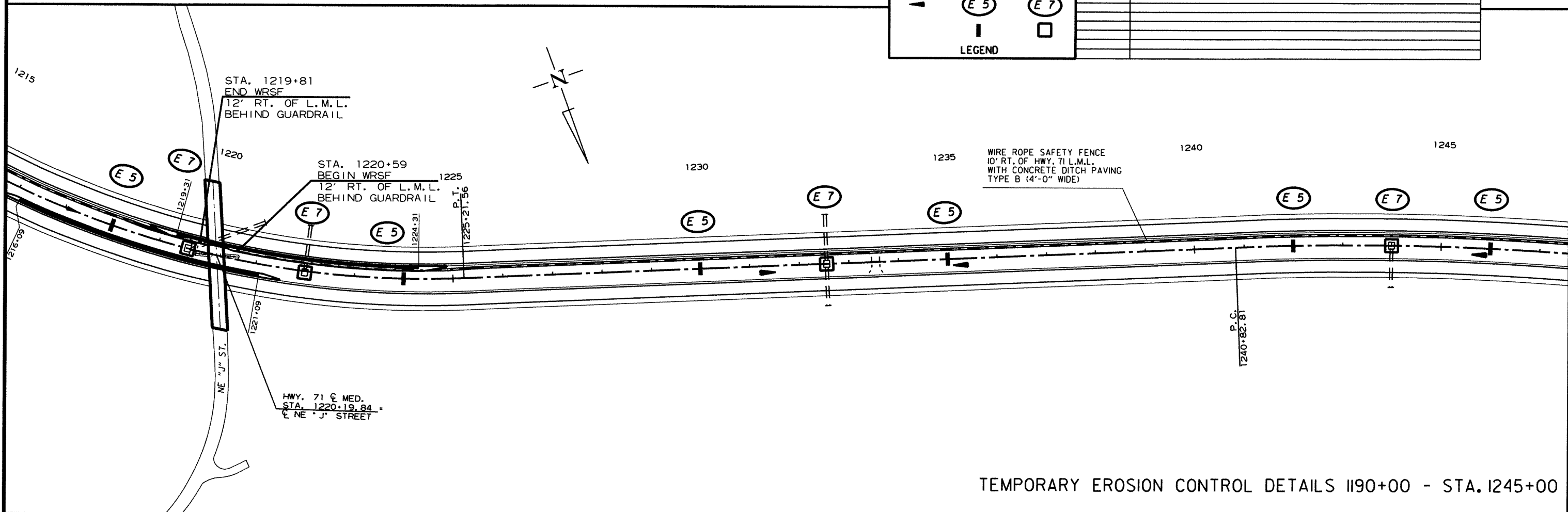
PI 1214+81.55
 DELTA = 75°49'14" LT.
 D = 3°00'00"
 T = 1487.34'
 L = 2527.35'
 PC = 1199+94.21
 PT = 1225+21.56

REVISION BOX

DATE OF REVISION	REVISION

FLOW DIRECTION	SAND BAG DITCH CHECK	DROP INLET SILT FENCE
←	(E 5)	(E 7)
		□

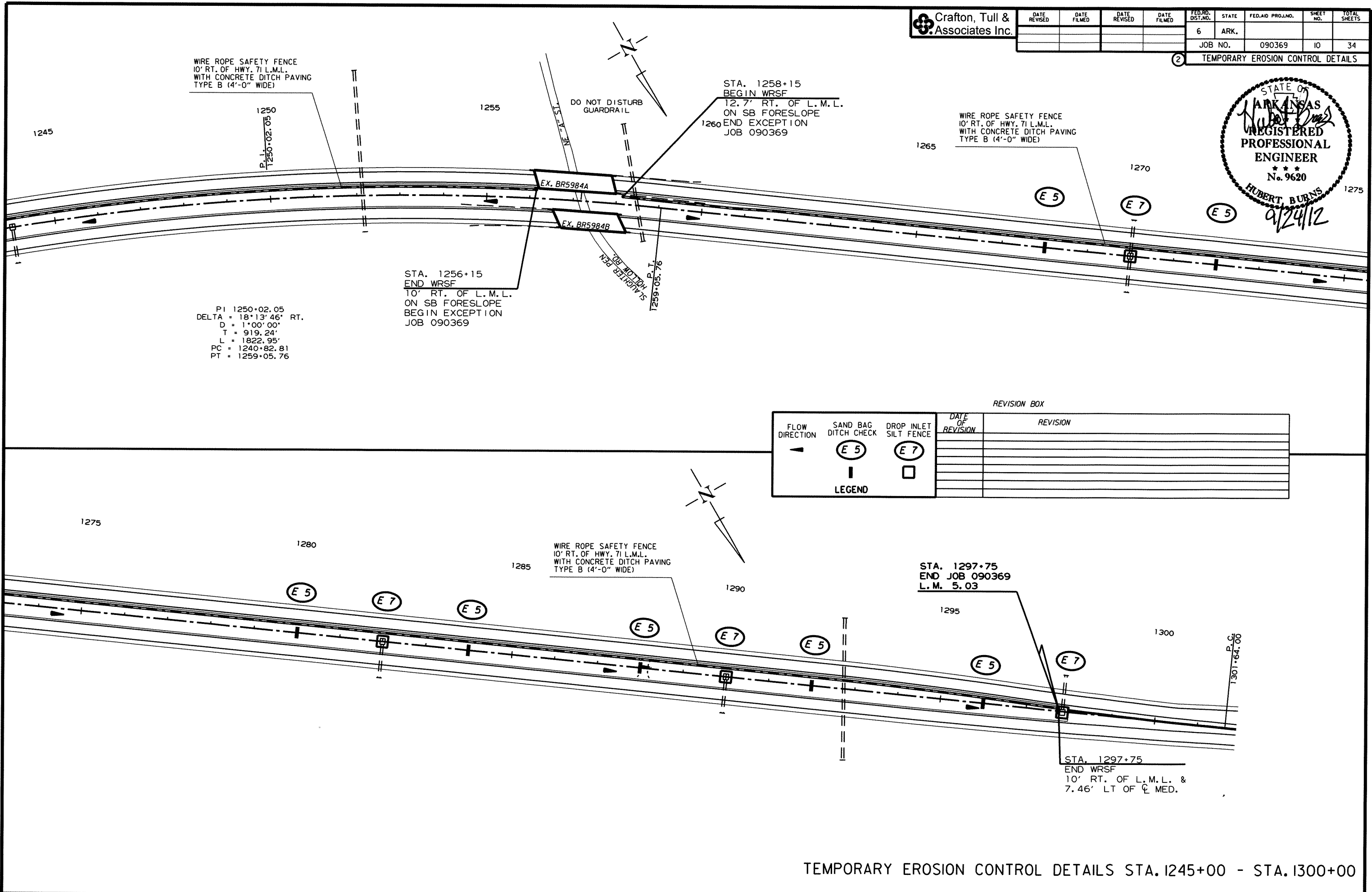
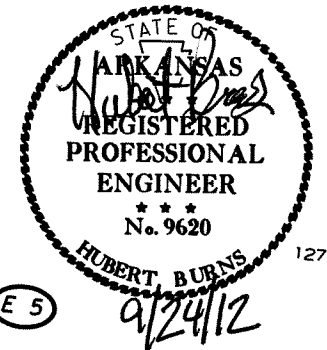
LEGEND



TEMPORARY EROSION CONTROL DETAILS 1190+00 - STA. 1245+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090369		10	34

2 TEMPORARY EROSION CONTROL DETAILS



WIRE ROPE SAFETY FENCE
10' RT. OF HWY. 71 L.M.L.
WITH CONCRETE DITCH PAVING
TYPE B (4'-0" WIDE)

STA. 1258+15
BEGIN WRSF
12.7' RT. OF L.M.L.
ON SB FORESLOPE
END EXCEPTION
JOB 090369

WIRE ROPE SAFETY FENCE
10' RT. OF HWY. 71 L.M.L.
WITH CONCRETE DITCH PAVING
TYPE B (4'-0" WIDE)

STA. 1256+15
END WRSF
10' RT. OF L.M.L.
ON SB FORESLOPE
BEGIN EXCEPTION
JOB 090369

PI 1250+02.05
DELTA = 18°13'46" RT.
D = 1'00'00"
T = 919.24'
L = 1822.95'
PC = 1240+82.81
PT = 1259+05.76

DATE OF REVISION	REVISION

STA. 1297+75
END JOB 090369
L.M. 5.03

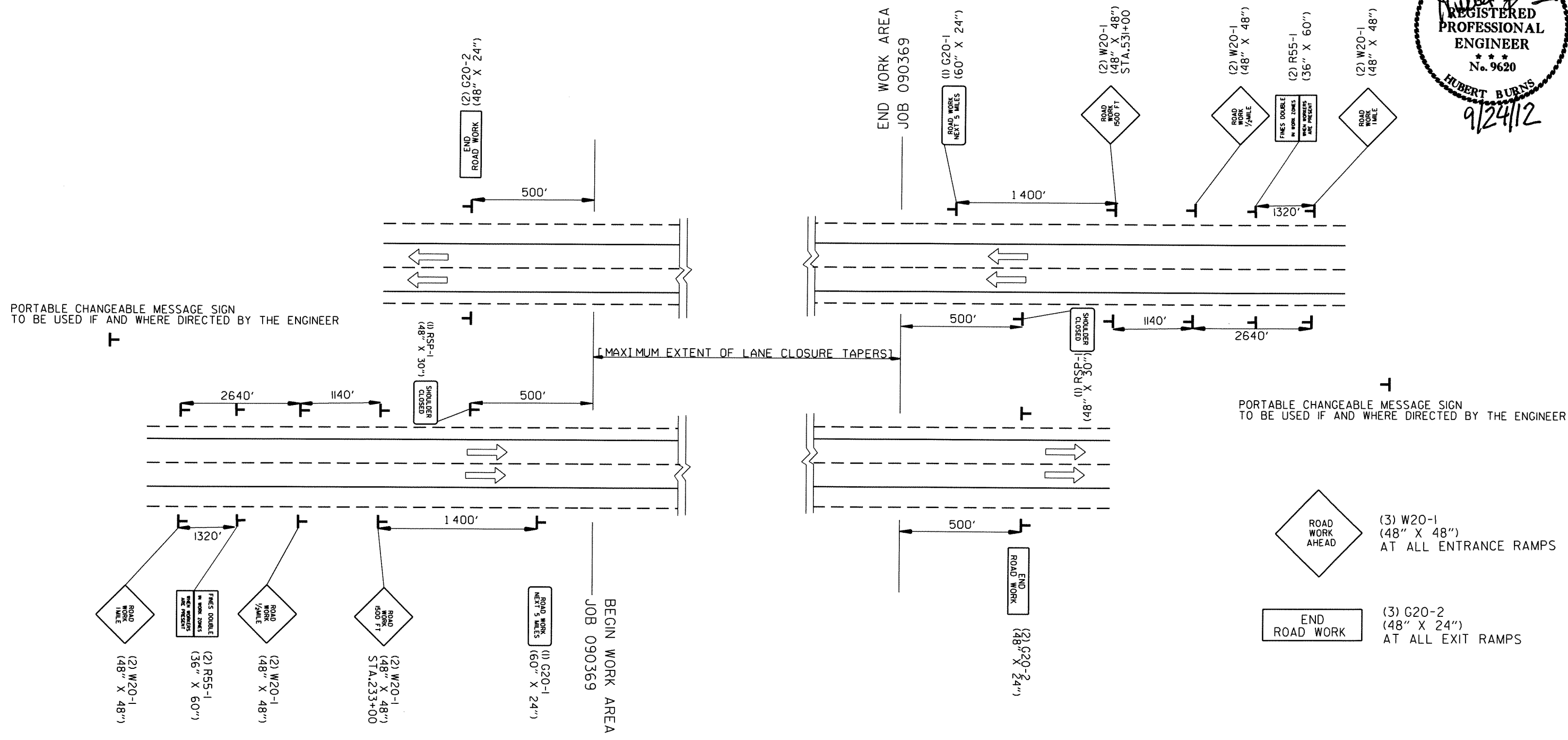
STA. 1297+75
END WRSF
10' RT. OF L.M.L. &
7.46' LT OF E. MED.

TEMPORARY EROSION CONTROL DETAILS STA. 1245+00 - STA. 1300+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090369	II	34	

2 MAINTENANCE OF TRAFFIC

NOTE :
W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS
TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS
AS WORKING AREA SHIFTS.



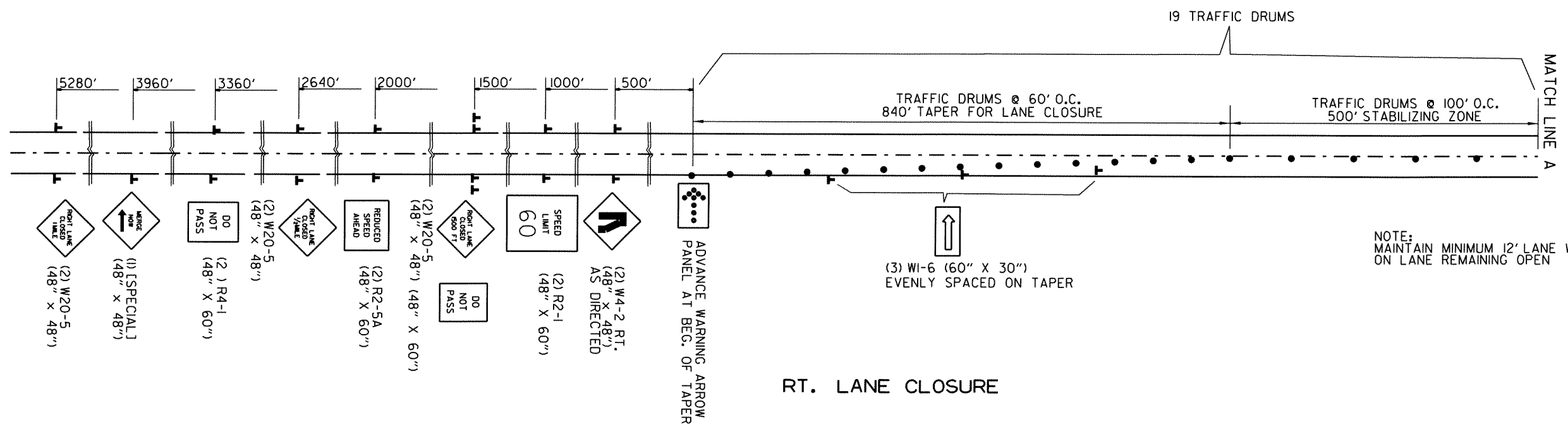
NOTE :
W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS
TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS
AS WORKING AREA SHIFTS.

ADVANCE SIGNS AT BEGINNING AND END OF JOB
ALL STAGES

MAINTENANCE OF TRAFFIC
ADVANCE SIGNS AT JOB ENDS

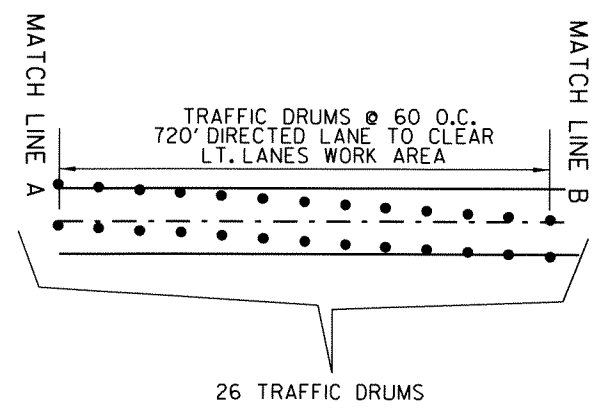
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090369	12	34	

2 MAINTENANCE OF TRAFFIC



NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN

RT. LANE CLOSURE



DIVERSION FOR LT. LANE WORK ZONE

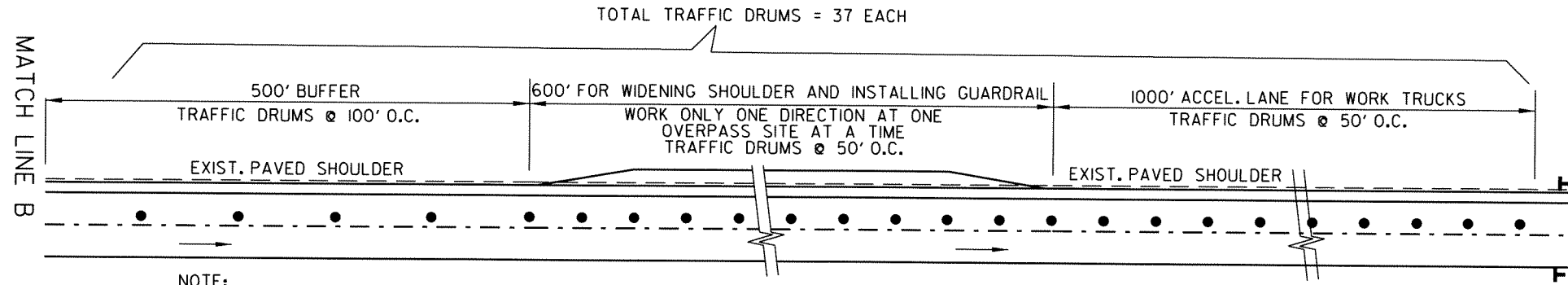
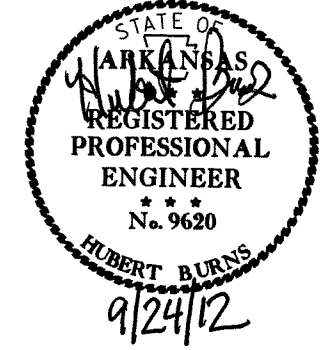
NOTE: REFER TO SP-MAINTENANCE OF TRAFFIC FOR LANE CLOSURE LIMITATIONS AND RESTRICTIONS. QUANTITY OF TRAFFIC DRUMS PROVIDED IN THE CONTRACT IS THE MAXIMUM NUMBER REQUIRED FOR ONE LANE CLOSURE.

NOTE: ANY WORK ZONE OUTSIDE THE LIMITS OF THE LANE CLOSURE AREA MUST HAVE PRIOR WRITTEN APPROVAL OF THE ENGINEER AND ANY ADDITIONAL TRAFFIC CONTROL DEVICES REQUIRED SHALL BE PROVIDED AT NO ADDITIONAL COST TO THE DEPARTMENT.

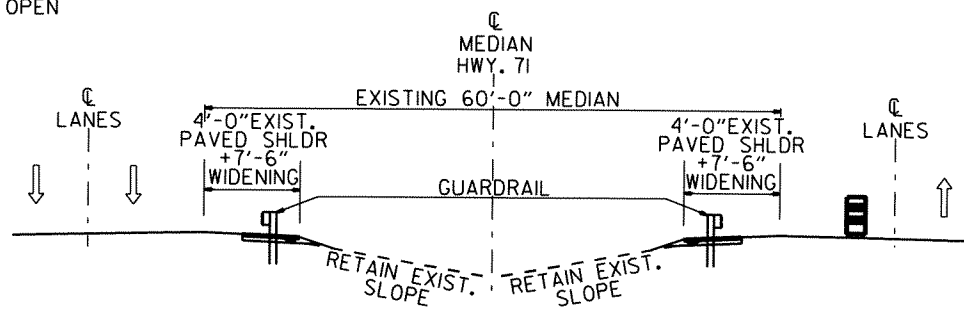
CABLE MEDIAN BARRIER MAINTENANCE OF TRAFFIC LANE CLOSURE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 090369							13	34

2 MAINTENANCE OF TRAFFIC



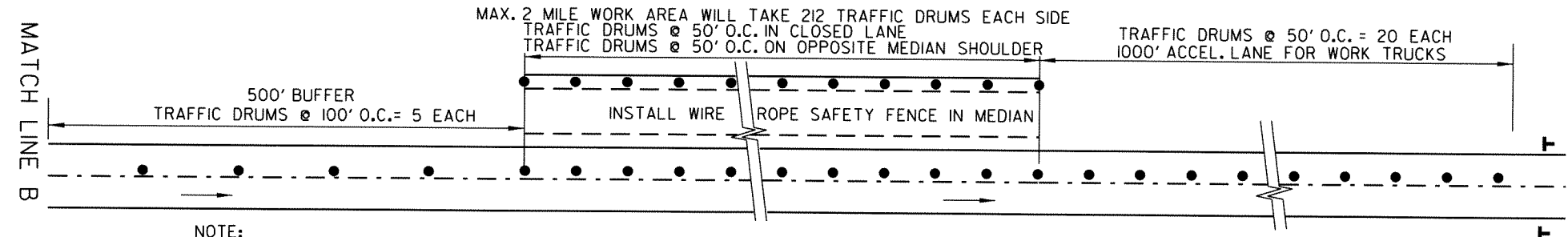
NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN



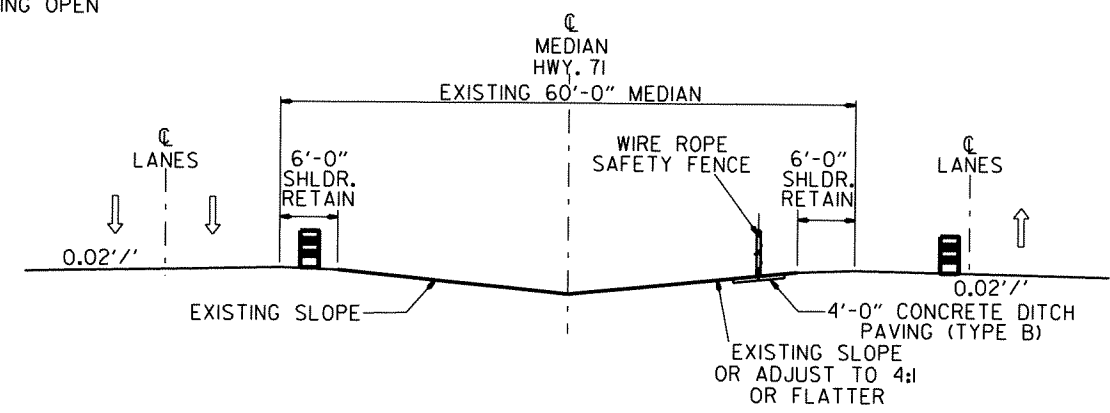
TRUCKS	SPEED LIMIT
65	70
(48" X 48") WHEN POSTED SPEED IS 70	(2) R2-1 (48" X 60") (2) R2-2

MOVABLE WORK ZONE FOR GUARDRAIL INSTALLATION

NOTE: REFER TO SP-MAINTENANCE OF TRAFFIC FOR LANE CLOSURE LIMITATIONS AND RESTRICTIONS. QUANTITY OF TRAFFIC DRUMS PROVIDED IN THE CONTRACT IS THE MAXIMUM NUMBER REQUIRED FOR ONE LANE CLOSURE.



NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN



TRUCKS	SPEED LIMIT
65	70
(48" X 48") WHEN POSTED SPEED IS 70	(2) R2-1 (48" X 60") (2) R2-2

MOVABLE WORK ZONE FOR WRSF INSTALLATION

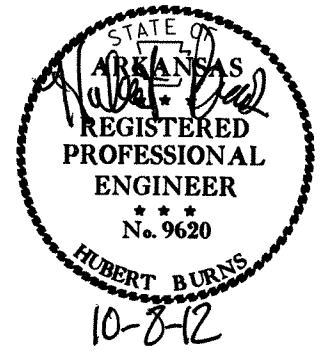
CABLE MEDIAN BARRIER MAINTENANCE OF TRAFFIC WORK AREAS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		090369	14	34

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS EACH	ADVANCE WARNING ARROW PANEL DAY	PORTABLE CHANGEABLE MESSAGE SIGN WEEK
				NO.	SQ. FT.			
W20-1	ROAD WORK 1 MILE	48"x48"	4	4	64.0			
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	64.0			
W20-1	ROAD WORK 1500 FT.	48"x48"	4	4	64.0			
W20-1	ROAD WORK AHEAD	48"x48"	3	3	48.0			
G20-2	END ROAD WORK	48"x24"	7	7	56.0			
G20-1	ROAD WORK NEXT 5 MILES	60"x24"	2	2	20.0			
SPECIAL 1	MERGE NOW + ARROW	48"x48"	1	1	16.0			
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1500 FEET	48"x48"	2	2	32.0			
W4-2R	RIGHT LANE CLOSING GRAPHIC	48"x48"	2	2	32.0			
W1-6	LARGER ARROW	30"x60"	3	3	37.5			
R4-1	DO NOT PASS	48"x60"	4	4	80.0			
R55-1	FINES DOUBLE IN WORK ZONES	36"x60"	4	4	60.0			
R2-5A	REDUCED SPEED AHEAD	48"x60"	2	2	40.0			
R2-1	SPEED LIMIT 60 MPH	48"x60"	2	2	40.0			
R2-1	SPEED LIMIT 70 MPH	48"x60"	2	2	40.0			
R2-2	TRUCKS SPEED LIMIT 65 MPH	48"x48"	2	2	32.0			
RSP-1	SHOULDER CLOSED	48"x30"	2	2	20.0			
	TRAFFIC DRUMS		494			494		
	ADVANCE WARNING ARROW PANEL		1				45	
	PORTABLE CHANGEABLE MESSAGE SIGN		2					40
TOTALS:					809.5	494	45	40

② QUANTITIES



THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2003 EDITION.

EROSION CONTROL

LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL					
	SEEDING	LIME	MULCH COVER	WATER	SECOND SEEDING APPLICATION	TEMPORARY SEEDING	MULCH COVER	WATER	SAND BAG DITCH CHECKS (E-5)	DROP INLET SILT FENCE (E-7)	* SEDIMENT REMOVAL AND DISPOSAL
	ACRE	TON	ACRE	M. GAL.	ACRE	ACRE	ACRE	M. GAL.	BAG	LIN. FT.	CU. YD.
ENTIRE JOB	3.63	7.26	3.63	370.3	3.63	3.63	3.63	74.1	820	414	61
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	3.00	6.00	3.00	306.0	3.00				200	225	
TOTALS:	6.63	13.26	6.63	676.3	6.63	3.63	3.63	74.1	1020	639	61

BASIS OF ESTIMATE:
 LIME.....2 TONS / ACRE OF SEEDING
 WATER.....102.0 M.G./ACRE OF SEEDING
 WATER..... 20.4 M.G./ACRE OF TEMPORARY SEEDING
 SAND BAG DITCH CHECKS.....20 BAGS / LOCATION
 DROP INLET SILT FENCE.....18 LIN.FT./LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ARE ESTIMATED. SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS.

QUANTITIES

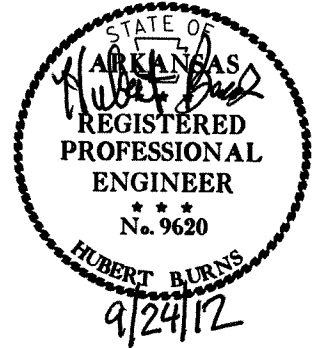
REMOVAL AND DISPOSAL ITEMS

STATION	DESCRIPTION	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIER	REMOVAL AND DISPOSAL OF PIPE CULVERTS
		EACH	EACH
1089+70	ON CL MEDIAN	2	
1137+25	ON CL MEDIAN	2	
1220+20	ON CL MEDIAN	2	
1233+54	C.M. PIPE CULVERT		1
1288+10	C.M. PIPE CULVERT		1
TOTALS:		6	2

NOTE: IMPACT ATTENUATION BARRIERS ARE TO REMAIN IN PLACE UNTIL THE GUARDRAIL IS INSTALLED.

GUARDRAIL

STATION	STATION	SIDE	GUARDRAIL (TYPE A)	TERMINAL ANCHOR POST (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)
			LIN. FT.	EACH	EACH
1085+60	1090+60	RT OF CL	450	1	1
1088+82	1093+82	LT OF CL	450	1	1
1133+09	1138+34	RT OF CL	475	1	1
1136+17	1141+42	LT OF CL	475	1	1
1216+09	1221+09	RT OF CL	450	1	1
1219+31	1224+31	LT OF CL	450	1	1
TOTALS:			2750	6	6



EARTHWORK

STATION	STATION	LOCATION	UNCLASSIFIED EXCAVATION	COMPACTED EMBANKMENT
			CU. YD.	CU. YD.
1085+02	1091+03	RT. SHLDR. WIDENING FOR GUARDRAIL AT BATTLEFIELD BLVD. OVERPASS	174	
1088+39	1094+40	LT. SHLDR. WIDENING FOR GUARDRAIL AT BATTLEFIELD BLVD. OVERPASS	174	
1132+66	1138+77	RT. SHLDR. WIDENING FOR GUARDRAIL AT HWY. 72 OVERPASS	94	
1135+59	1142+00	LT. SHLDR. WIDENING FOR GUARDRAIL AT HWY. 72 OVERPASS	192	
1215+66	1221+52	RT. SHLDR. WIDENING FOR GUARDRAIL AT NE "J" ST. OVERPASS	90	
1218+73	1224+89	LT. SHLDR. WIDENING FOR GUARDRAIL AT NE "J" ST. OVERPASS	196	
ENTIRE PROJECT		GRAVEL MEDIAN CROSSING REMOVALS	95	
IF AND WHERE DIRECTED BY THE ENGINEER				500 *
TOTALS:			1015	500

NOTE: EARTHWORK QUANTITIES AT THE LOCATIONS SHOWN ABOVE SHALL BE PAID FOR AS PLAN QUANTITY. * QUANTITIES ARE ESTIMATED. SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS. COMPACTION WILL BE AT THE SATISFACTION OF THE ENGINEER.

CONCRETE DITCH PAVING

STATION	STATION	LOCATION	LENGTH	WIDTH	CONC. DITCH PAVING (TYPE B)	SOLID SODDING	*WATER
			FEET	FEET	SQ. YD.		M. GAL.
1032+00.00	1043+50.00	HWY. 71 CL MEDIAN RT	1150	4	511	256	3.2
1045+65.00	1085+50.00	HWY. 71 CL MEDIAN RT	3985	4	1771	886	11.2
1093+92.00	1136+07.00	HWY. 71 CL MEDIAN LT	4215	4	1873	937	11.8
1141+52.00	1219+21.00	HWY. 71 CL MEDIAN LT	7769	4	3453	1726	21.7
1224+41.00	1256+15.00	HWY. 71 CL MEDIAN LT	3174	4	1411	705	8.9
1258+15.00	1297+75.00	HWY. 71 CL MEDIAN LT	3960	4	1760	880	11.1
TOTALS:					10779	5390	67.9

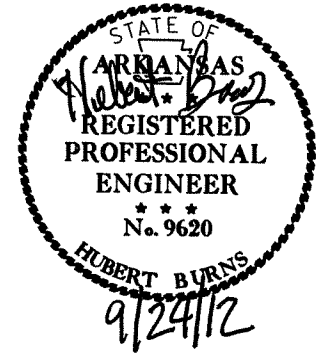
BASIS OF ESTIMATE:
WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING.

WIRE ROPE SAFETY FENCE

STATION	STATION	LOCATION	WIRE ROPE SAFETY FENCE	WRSF ANCHOR *	WRSF MAINTENANCE MATERIALS
			LIN. FT.	EACH	LUMP SUM
1032+00.00	1043+50.00	HWY. 71 CL MEDIAN RT	1150	2	
1045+65.00	1089+32.00	HWY. 71 CL MEDIAN RT	4367	2	
1090+10.00	1136+67.00	HWY. 71 CL MEDIAN LT	4657	2	
1137+84.00	1219+81.00	HWY. 71 CL MEDIAN LT	8197	2	
1220+59.00	1256+15.00	HWY. 71 CL MEDIAN LT	3556	2	
1258+15.00	1297+75.00	HWY. 71 CL MEDIAN LT	3960	2	
ENTIRE PROJECT					1.00
TOTALS:			25887	12	1.00

* THIS ITEM SHOWN FOR INFORMATION ONLY.

2 QUANTITIES



BASE AND SURFACING

STATION	STATION	LOCATION	LENGTH FEET	AGGREGATE BASE COURSE (CLASS 7)		ACHM SURFACE COURSE (1/2") (PG 64-22)			
				TON / STATION	TON	AVG. WID. FEET	SQ. YD.	POUND / SQ. YD.	TON
1085+02	1085+50	RT SHLDR TAPER	48.0	27.1	13.0	6.0	32.0	220.0	3.5
1085+50	1089+42	RT SHLDR FULL WIDTH WIDENING	392.0	40.8	159.9	10.0	435.6	220.0	47.9
1089+42	1089+57	RT SHLDR TAPER	15.0	37.9	5.7	8.8	14.7	220.0	1.6
1089+57	1090+70	RT SHLDR FULL WIDTH WIDENING	113.0	35.0	39.6	7.5	94.2	220.0	10.4
1090+70	1091+03	RT SHLDR TAPER	33.0	24.3	8.0	4.8	17.6	220.0	1.9
1088+39	1088+72	LT SHLDR TAPER	33.0	24.3	8.0	4.8	17.6	220.0	1.9
1088+72	1089+85	LT SHLDR FULL WIDTH WIDENING	113.0	35.0	39.6	7.5	94.2	220.0	10.4
1089+85	1090+00	LT SHLDR TAPER	15.0	37.9	5.7	8.8	14.7	220.0	1.6
1090+00	1093+92	LT SHLDR FULL WIDTH WIDENING	392.0	40.8	159.9	10.0	435.6	220.0	47.9
1093+92	1094+40	LT SHLDR TAPER	48.0	27.1	13.0	6.0	32.0	220.0	3.5
1132+66	1132+99	RT SHLDR TAPER	33.0	24.3	8.0	4.8	17.6	220.0	1.9
1132+99	1138+44	RT SHLDR FULL WIDTH WIDENING	545.0	35.0	190.8	7.5	454.2	220.0	50.0
1138+44	1138+77	RT SHLDR TAPER	33.0	24.3	8.0	4.8	17.6	220.0	1.9
1135+59	1136+07	LT SHLDR TAPER	48.0	27.1	13.0	6.0	32.0	220.0	3.5
1136+07	1136+77	LT SHLDR FULL WIDTH WIDENING	70.0	40.8	28.6	10.0	77.8	220.0	8.6
1136+77	1136+92	LT SHLDR TAPER	15.0	37.9	5.7	8.8	14.7	220.0	1.6
1136+92	1137+59	LT SHLDR FULL WIDTH WIDENING	67.0	35.0	23.5	7.5	55.8	220.0	6.1
1137+59	1137+74	LT SHLDR TAPER	15.0	37.9	5.7	8.8	14.7	220.0	1.6
1137+74	1141+52	LT SHLDR FULL WIDTH WIDENING	378.0	40.8	154.2	10.0	420.0	220.0	46.2
1141+52	1142+00	LT SHLDR TAPER	48.0	27.1	13.0	6.0	32.0	220.0	3.5
1215+66	1215+99	RT SHLDR TAPER	33.0	24.3	8.0	4.8	17.6	220.0	1.9
1215+99	1221+19	RT SHLDR FULL WIDTH WIDENING	520.0	35.0	182.0	7.5	433.3	220.0	47.7
1221+19	1221+52	RT SHLDR TAPER	33.0	24.3	8.0	4.8	17.6	220.0	1.9
1218+73	1219+21	LT SHLDR TAPER	48.0	27.1	13.0	6.0	32.0	220.0	3.5
1219+21	1219+91	LT SHLDR FULL WIDTH WIDENING	70.0	40.8	28.6	10.0	77.8	220.0	8.6
1219+91	1220+06	LT SHLDR TAPER	15.0	37.9	5.7	8.8	14.7	220.0	1.6
1220+06	1220+34	LT SHLDR FULL WIDTH WIDENING	28.0	35.0	9.8	7.5	23.3	220.0	2.6
1220+34	1220+49	LT SHLDR TAPER	15.0	37.9	5.7	8.8	14.7	220.0	1.6
1220+49	1224+41	LT SHLDR FULL WIDTH WIDENING	392.0	40.8	159.9	10.0	435.6	220.0	47.9
1224+41	1224+89	LT SHLDR TAPER	48.0	27.1	13.0	6.0	32.0	220.0	3.5
TOTALS:					1336.6		3423.2		376.3

BASIS OF ESTIMATE:
 ACHM SURFACE COURSE (1/2").....94.6% MIN. AGGR.....5.4% ASPHALT BINDER
 MAXIMUM NUMBER OF GYRATIONS = 115

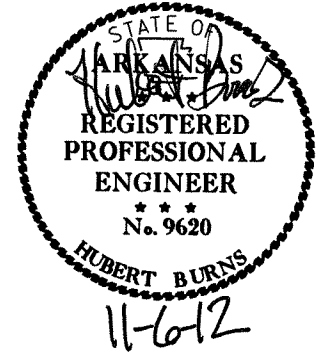
QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
11-06-12				6	ARK.			
				JOB NO.	090369	17	34	

2 SUMMARY OF QUANTITIES & REVISIONS

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
202	REMOVAL AND DISPOSAL OF PIPE CULVERTS	2	EACH
210	UNCLASSIFIED EXCAVATION	1015	CU.YD.
210	COMPACTED EMBANKMENT	500	CU.YD.
SS & 303	AGGREGATE BASE COURSE (CLASS 7)	1337	TON
SP, SS & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	356	TON
SP, SS & 407	ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (1/2")	20	TON
601	MOBILIZATION	1.00	LUMP SUM
SP, SS & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	810	SQ.FT.
SS & 604	TRAFFIC DRUMS	494	EACH
SS & 604	ADVANCE WARNING ARROW PANEL	45	DAY
SP, SS & 604	PORTABLE CHANGEABLE MESSAGE SIGN	40	WEEK
SP & 605	CONCRETE DITCH PAVING (TYPE B)	10779	SQ.YD.
SS & 617	GUARDRAIL (TYPE A)	2750	LIN.FT.
SS & 617	TERMINAL ANCHOR POSTS (TYPE 1)	6	EACH
SS & 617	GUARDRAIL TERMINAL (TYPE 2)	6	EACH
620	LIME	13	TON
620	SEEDING	6.63	ACRE
620	MULCH COVER	10.26	ACRE
SS & 620	WATER	818.3	M.GAL.
621	TEMPORARY SEEDING	3.63	ACRE
621	SAND BAG DITCH CHECKS	1020	BAG
621	DROP INLET SILT FENCE	639	LIN.FT.
621	SEDIMENT REMOVAL AND DISPOSAL	61	CY.YD.
623	SECOND SEEDING APPLICATION	6.63	ACRE
624	SOLID SODDING	5390	SQ.YD.
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
SP	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIER	6	EACH
SP	WIRE ROPE SAFETY FENCE	25887	LIN.FT.
SP	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS	1.00	LUMP SUM

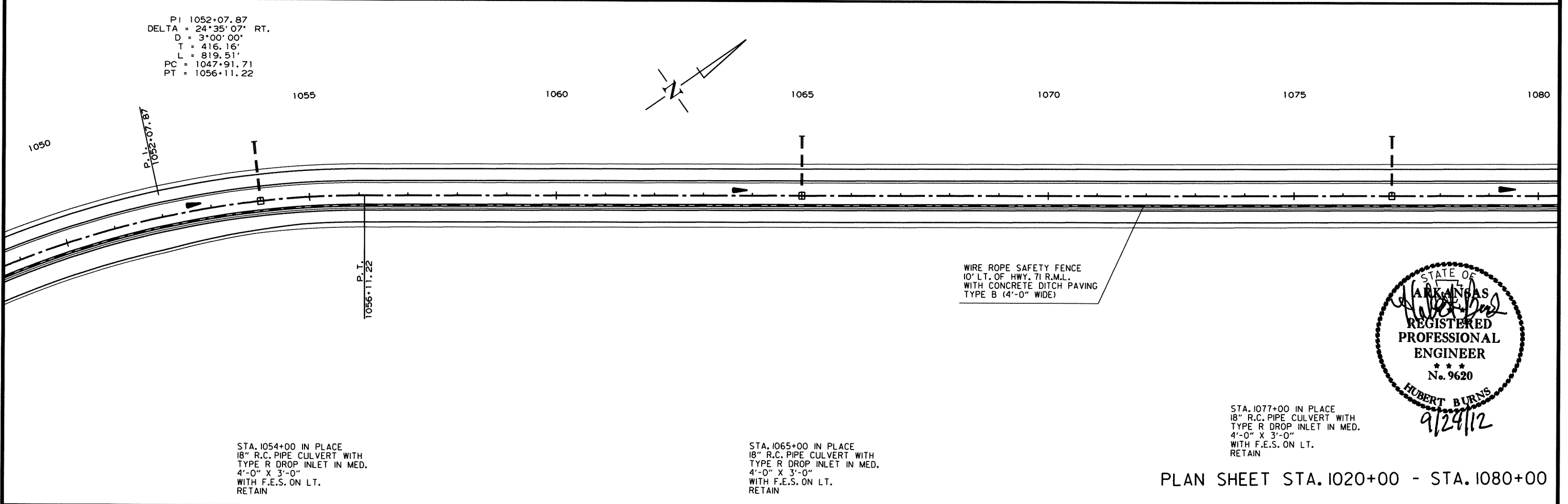
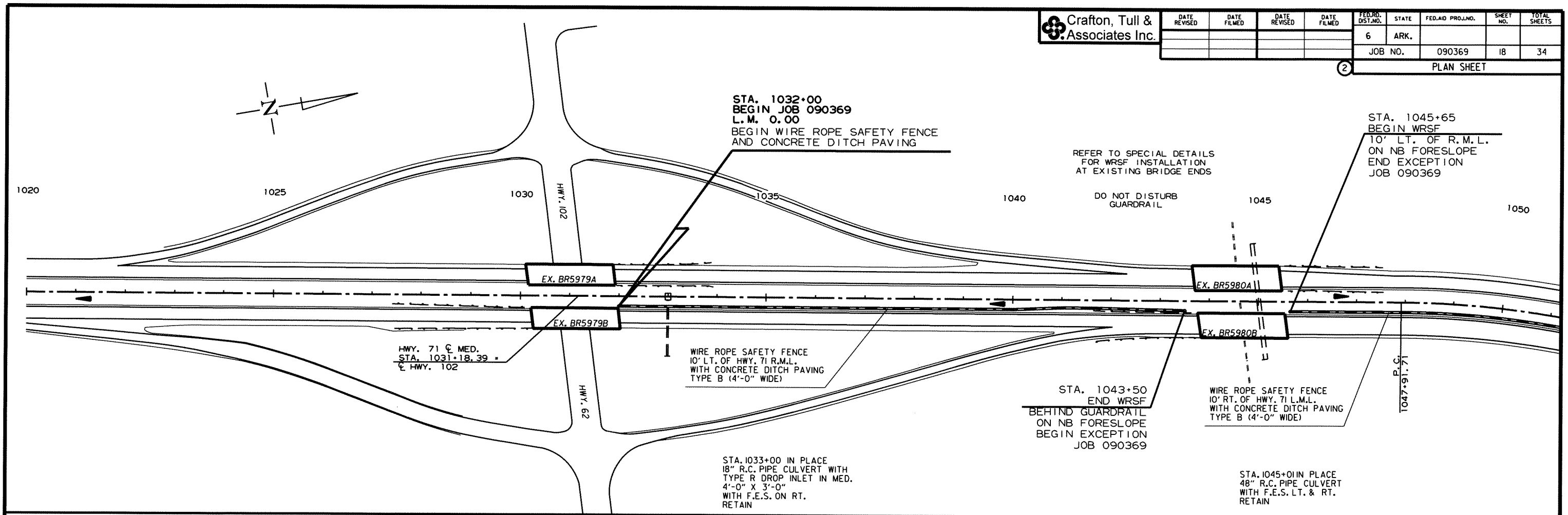


REVISIONS

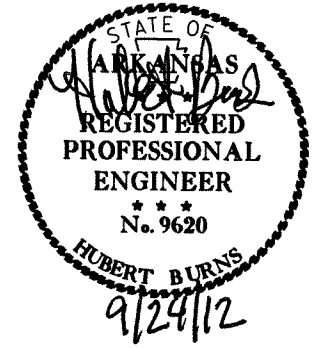
DATE	REVISION	SHEET NUMBER
11-06-12	ADDED SP "WIRE ROPE SAFETY FENCE PARTS"	2 & 17

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090369		18	34

2 PLAN SHEET



PI 1052+07.87
 DELTA = 24°35'07" RT.
 D = 3°00'00"
 T = 416.16'
 L = 819.51'
 PC = 1047+91.71
 PT = 1056+11.22



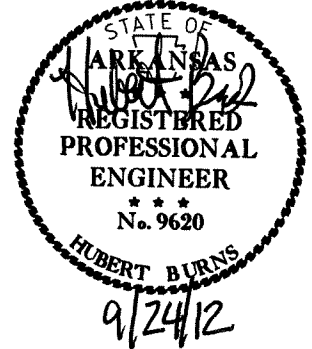
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PLAN SHEET STA. 1020+00 - STA. 1080+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						090369	19	34

STA	STA	GUARDRAIL (TYPE A)	ANCHOR POST (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)
1085+60	1090+60	MEDIAN RT SHLDR--450 LIN. FT.--1 EACH	----	1 EACH
1088+82	1093+82	MEDIAN LT SHLDR--450 LIN. FT.--1 EACH	----	1 EACH

PI 1108+43.45
 DELTA = 81°01'14" LT.
 D = 2°30'00"
 T = 1958.12'
 L = 3240.82'
 PC = 1088+85.33
 PT = 1121+26.15



REFER TO SPECIAL DETAILS FOR WRSF INSTALLATION BEHIND EXISTING GUARDRAIL AT OVERPASS PIERS

STA. 1089+70 IN PLACE (2) TYPE-B IMPACT ATTENUATION BARRIERS REMOVE

WIRE ROPE SAFETY FENCE 10' RT. OF HWY. 71 R.M.L. WITH CONCRETE DITCH PAVING TYPE B (4'-0" WIDE)

STA. 1090+10 END WRSF 12' RT. OF L.M.L. BEHIND GUARDRAIL

WIRE ROPE SAFETY FENCE 10' LT. OF HWY. 71 R.M.L. WITH CONCRETE DITCH PAVING TYPE B (4'-0" WIDE)

STA. 1089+32 END WRSF 12' LT. OF R.M.L. BEHIND GUARDRAIL

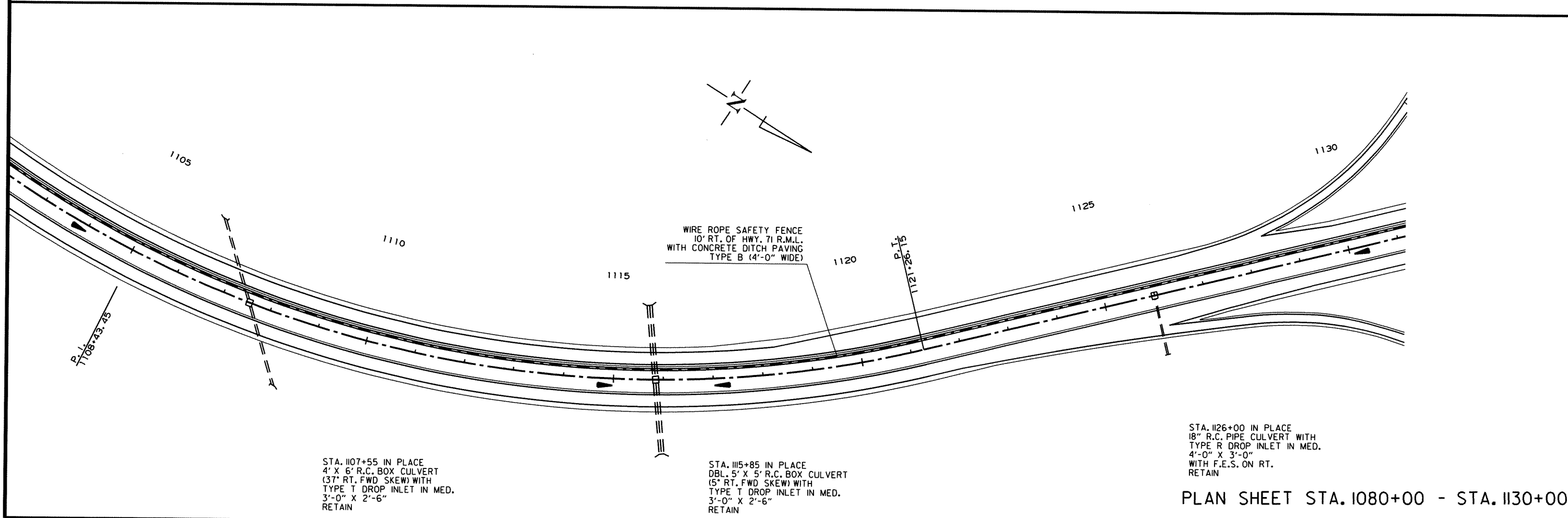
STA. 1087+00 IN PLACE 18" R.C. PIPE CULVERT WITH TYPE R DROP INLET IN MED. 4'-0" X 3'-0" WITH F.E.S. ON LT. RETAIN

STA. 1089+08 IN PLACE GRAVEL MEDIAN CROSSOVER IN MED. REMOVE

HWY. 71 C. MED. STA. 1089+70.69 C. BATTLEFIELD BLVD. (LITTLE FLOCK RD.)

STA. 1097+00 IN PLACE 18" R.C. PIPE CULVERT WITH TYPE R DROP INLET IN MED. 4'-0" X 3'-0" WITH F.E.S. ON LT. RETAIN

STA. 1099+00 IN PLACE WATER LINE CROSSING (APPROX. LOCATION) RETAIN



STA. 1107+55 IN PLACE 4' X 6' R.C. BOX CULVERT (37° RT. FWD SKEW) WITH TYPE T DROP INLET IN MED. 3'-0" X 2'-6" RETAIN

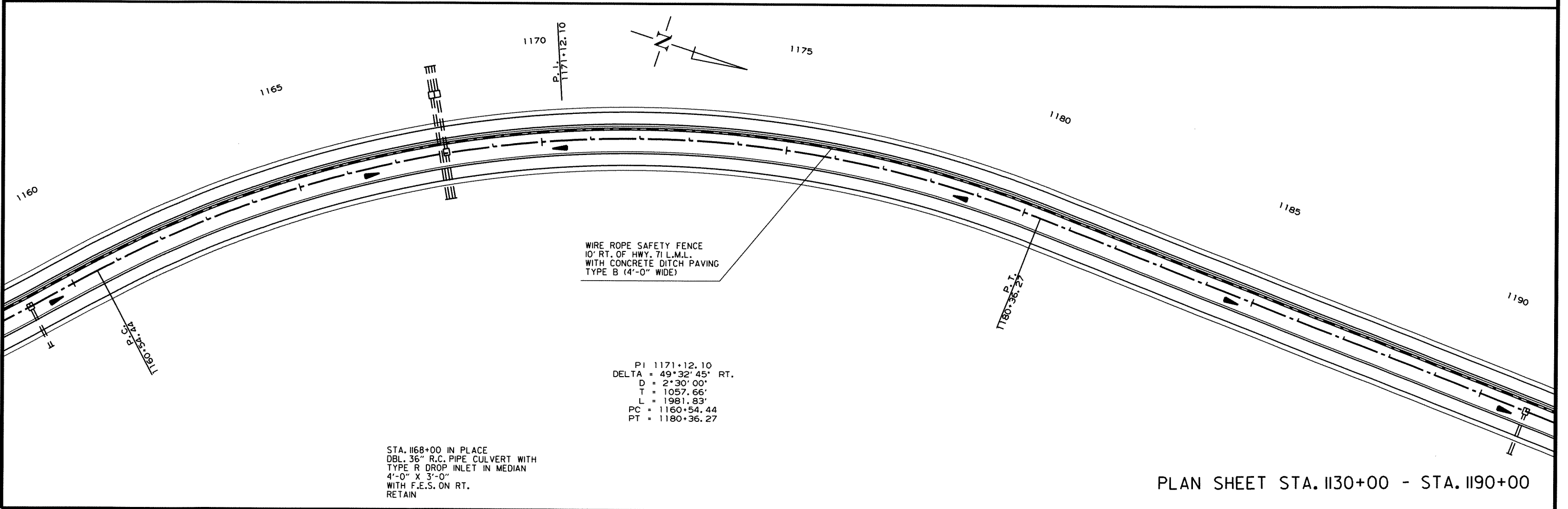
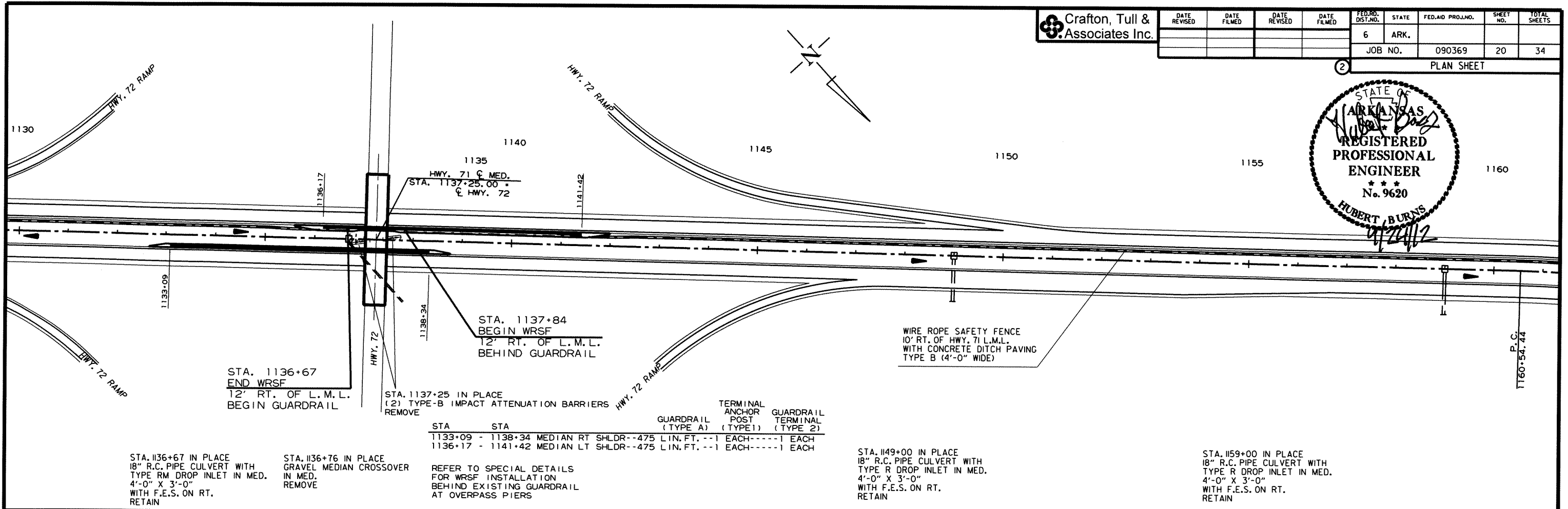
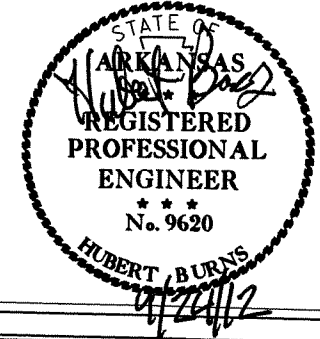
STA. 1115+85 IN PLACE DBL. 5' X 5' R.C. BOX CULVERT (5° RT. FWD SKEW) WITH TYPE T DROP INLET IN MED. 3'-0" X 2'-6" RETAIN

STA. 1126+00 IN PLACE 18" R.C. PIPE CULVERT WITH TYPE R DROP INLET IN MED. 4'-0" X 3'-0" WITH F.E.S. ON RT. RETAIN

PLAN SHEET STA. 1080+00 - STA. 1130+00

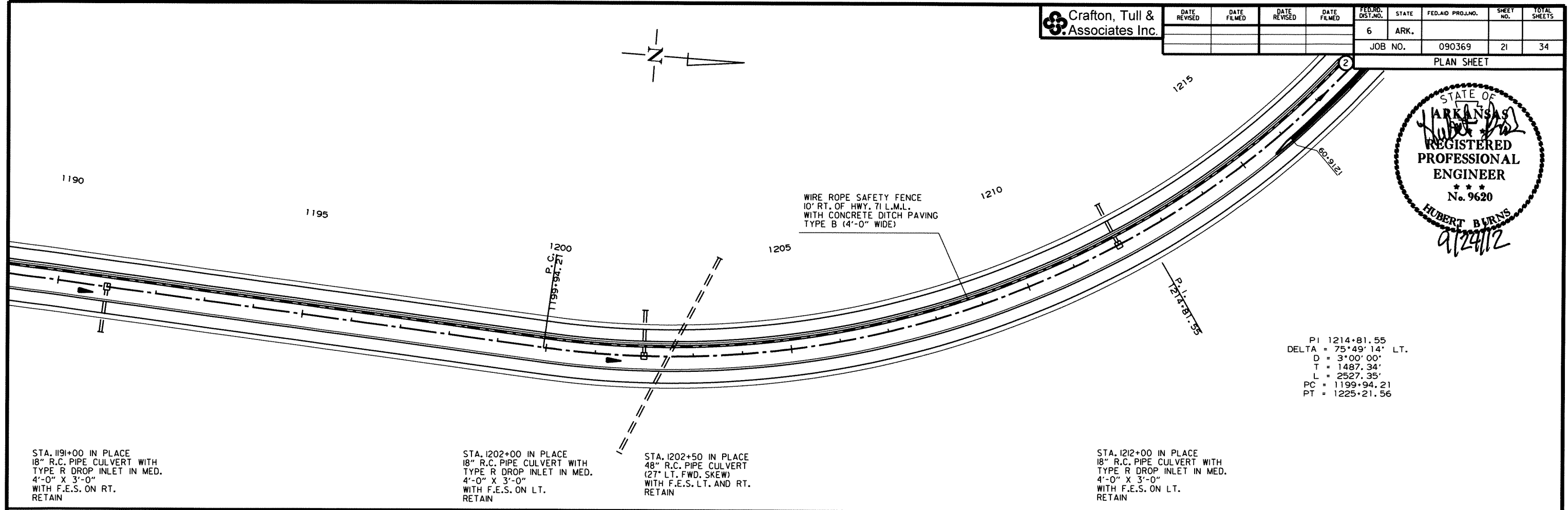
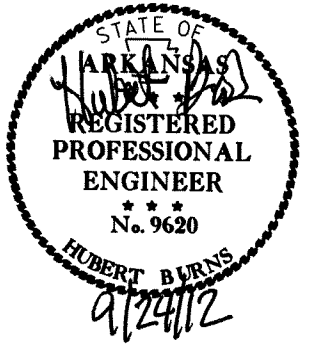
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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 090369							20	34
PLAN SHEET								



PLAN SHEET STA. 1130+00 - STA. 1190+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		21	34
				JOB NO.		090369		



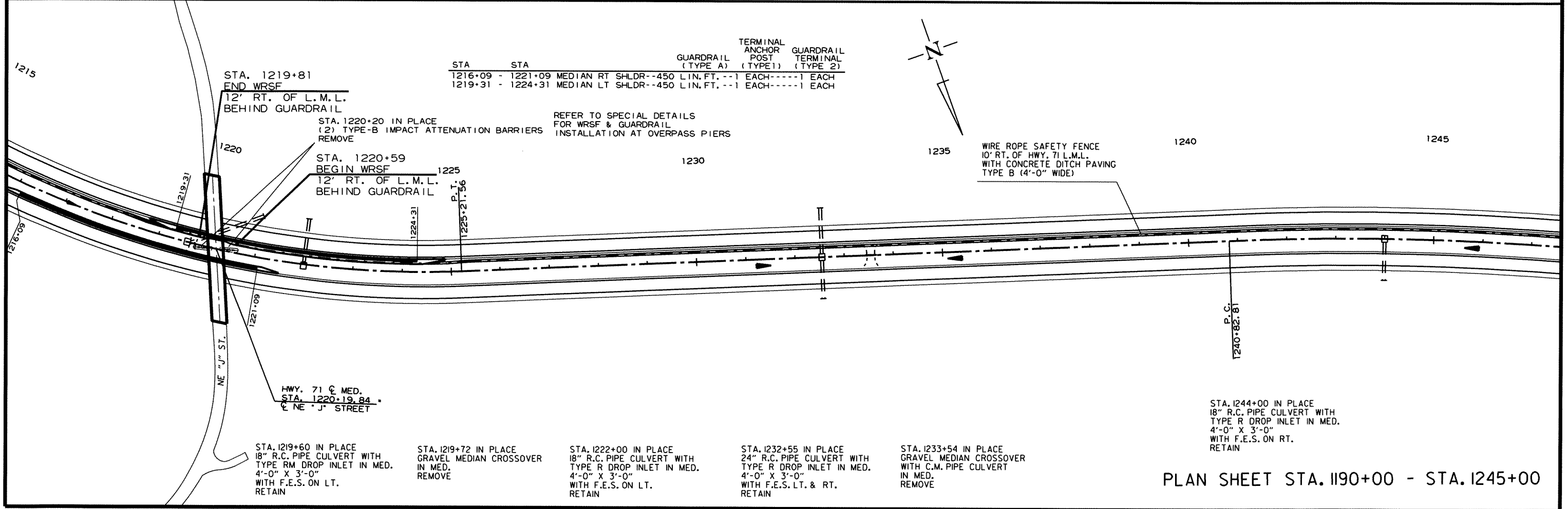
PI 1214+81.55
 DELTA = 75°49'14" LT.
 D = 3'00'00"
 T = 1487.34'
 L = 2527.35'
 PC = 1199+94.21
 PT = 1225+21.56

STA. 1191+00 IN PLACE
 18" R.C. PIPE CULVERT WITH
 TYPE R DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. ON RT.
 RETAIN

STA. 1202+00 IN PLACE
 18" R.C. PIPE CULVERT WITH
 TYPE R DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. ON LT.
 RETAIN

STA. 1202+50 IN PLACE
 48" R.C. PIPE CULVERT
 (27' LT. FWD. SKEW)
 WITH F.E.S. LT. AND RT.
 RETAIN

STA. 1212+00 IN PLACE
 18" R.C. PIPE CULVERT WITH
 TYPE R DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. ON LT.
 RETAIN



STA	STA	GUARDRAIL (TYPE A)	TERMINAL ANCHOR POST (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)
1216+09	1221+09	MEDIAN RT SHLDR--450 LIN. FT.--	1 EACH	1 EACH
1219+31	1224+31	MEDIAN LT SHLDR--450 LIN. FT.--	1 EACH	1 EACH

STA. 1219+81
 END WRSF
 12' RT. OF L.M.L.
 BEHIND GUARDRAIL

STA. 1220+20 IN PLACE
 (2) TYPE-B IMPACT ATTENUATION BARRIERS
 REMOVE

STA. 1220+59
 BEGIN WRSF
 12' RT. OF L.M.L.
 BEHIND GUARDRAIL

REFER TO SPECIAL DETAILS
 FOR WRSF & GUARDRAIL
 INSTALLATION AT OVERPASS PIERS

WIRE ROPE SAFETY FENCE
 10' RT. OF HWY. 71 L.M.L.
 WITH CONCRETE DITCH PAVING
 TYPE B (4'-0" WIDE)

STA. 1219+60 IN PLACE
 18" R.C. PIPE CULVERT WITH
 TYPE RM DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. ON LT.
 RETAIN

STA. 1219+72 IN PLACE
 GRAVEL MEDIAN CROSSOVER
 IN MED.
 REMOVE

STA. 1222+00 IN PLACE
 18" R.C. PIPE CULVERT WITH
 TYPE R DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. ON LT.
 RETAIN

STA. 1232+55 IN PLACE
 24" R.C. PIPE CULVERT WITH
 TYPE R DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. LT. & RT.
 RETAIN

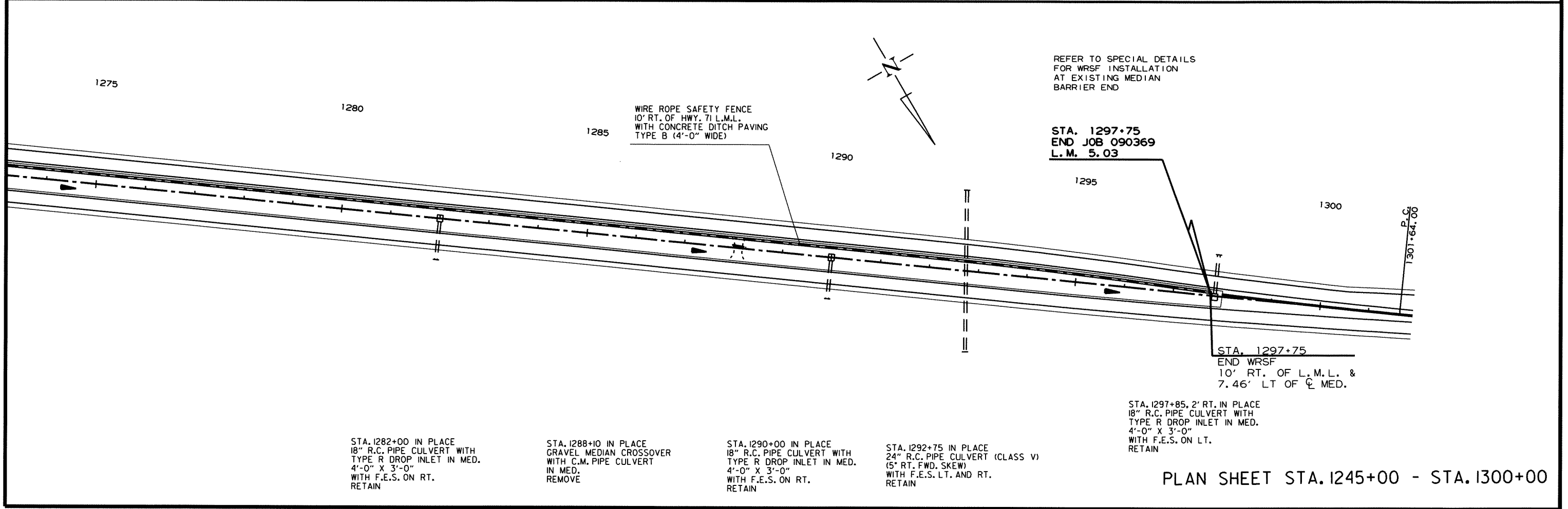
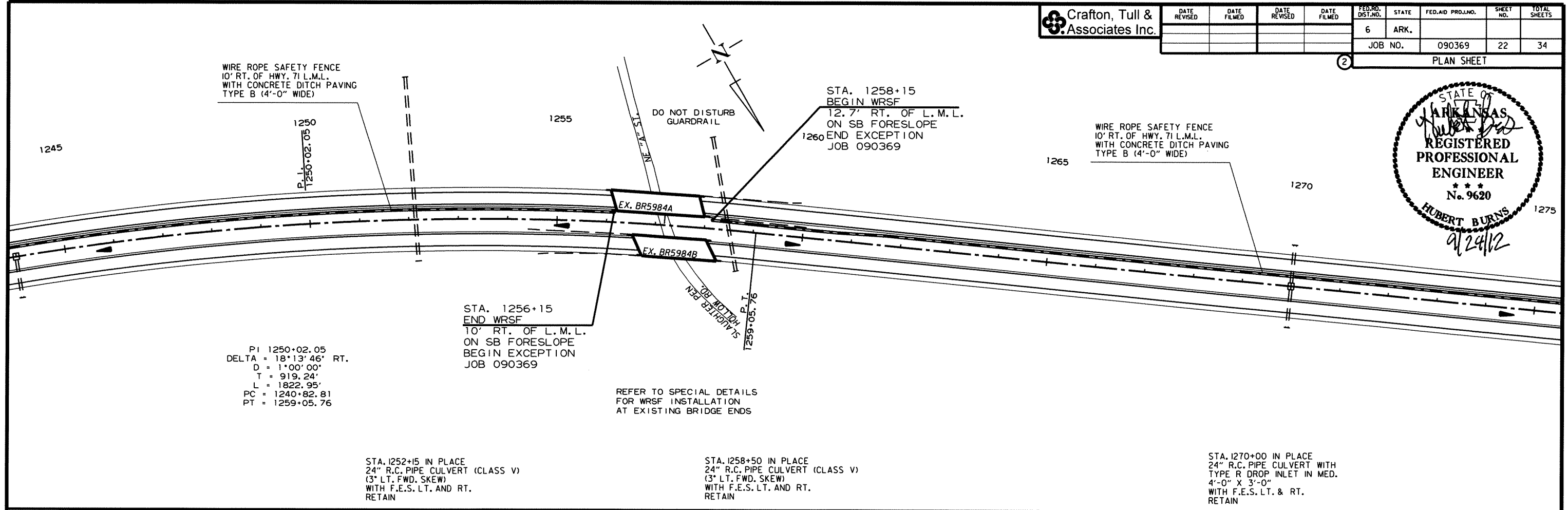
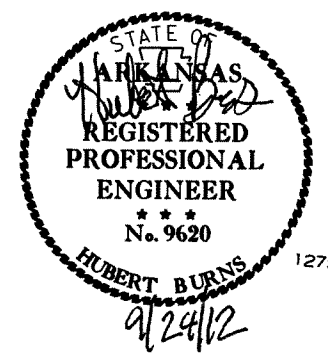
STA. 1233+54 IN PLACE
 GRAVEL MEDIAN CROSSOVER
 WITH C.M. PIPE CULVERT
 IN MED.
 REMOVE

STA. 1244+00 IN PLACE
 18" R.C. PIPE CULVERT WITH
 TYPE R DROP INLET IN MED.
 4'-0" X 3'-0"
 WITH F.E.S. ON RT.
 RETAIN

PLAN SHEET STA. 1190+00 - STA. 1245+00

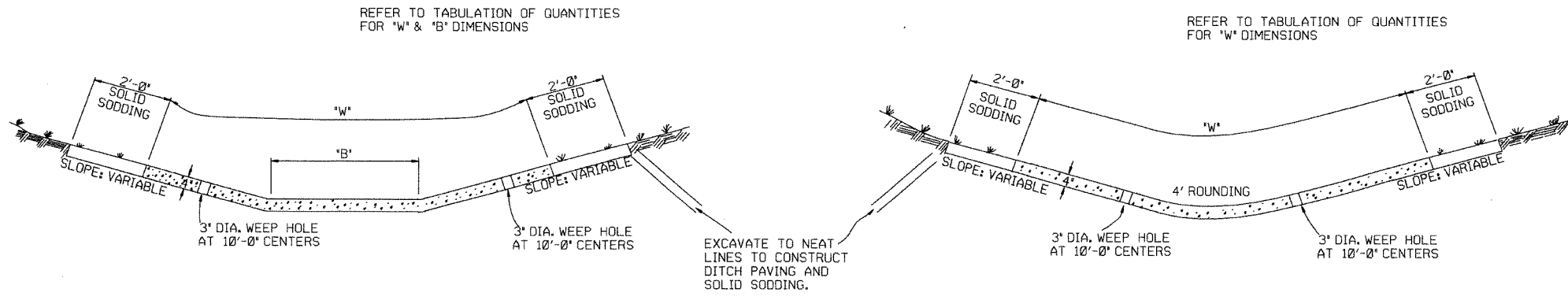
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		090369	22	34

② PLAN SHEET



USER: bc500
 DESIGN FILE: G:\12104802_Hwy71\TRANSP\dgn\090369.dgn
 PLOTTED: 9/21/2012 08:40
 SCALE: 200H

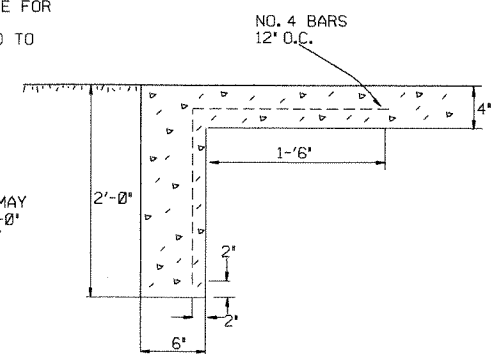
PLAN SHEET STA. 1245+00 - STA. 1300+00



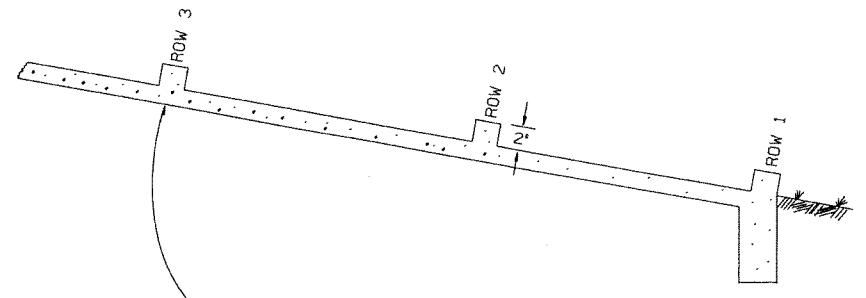
TYPE A

TYPE B

THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR 'CONCRETE DITCH PAVING.'

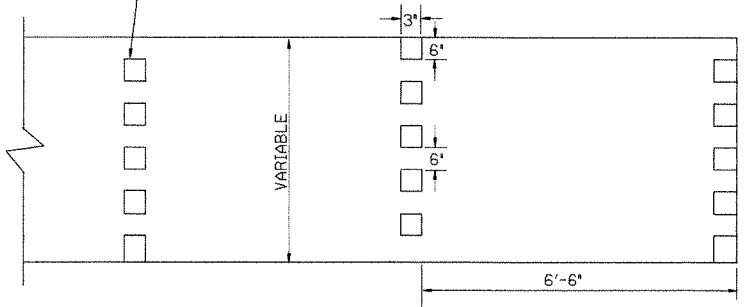


TOE WALL DETAIL FOR CONCRETE DITCH PAVING



NUMBER OF ELEMENTS PER ROW VARIES WITH WIDTH OF PAVING SPECIFIED

ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.



ENERGY DISSIPATORS
(NO SCALE)

GENERAL NOTES:

THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.

TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.

SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.

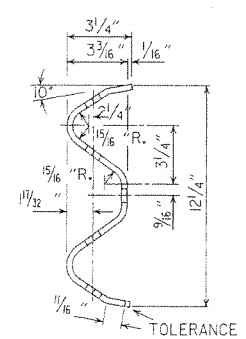
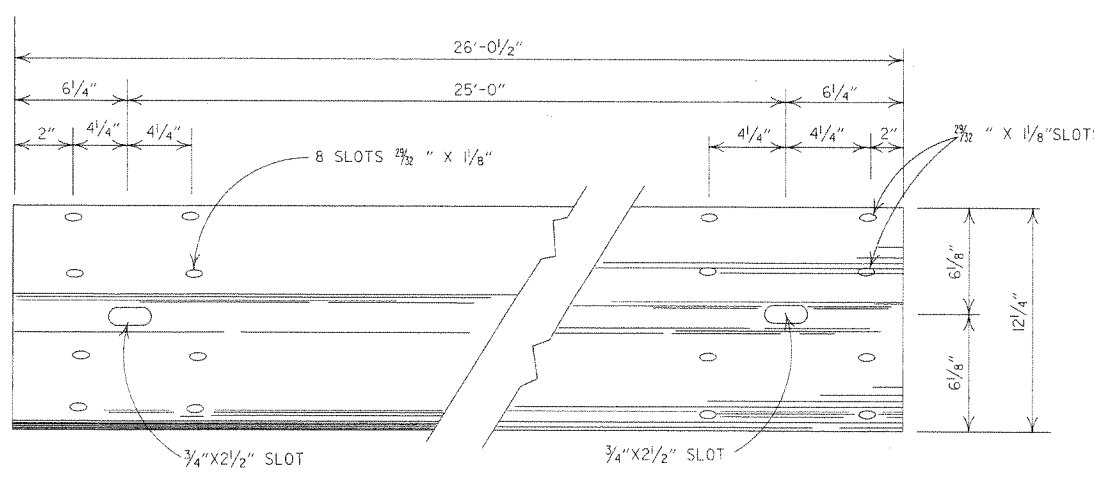
1" WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.

11-17-10	ADDED GENERAL NOTE	
6-2-94	ADDED GENERAL NOTE ABOUT SOLID SODDING	
11-30-8	ELIMINATED MIN. ROWS OF ELEMENTS	111-30-89
7-15-88	REVISED DISSIPATOR NOTE	653-7-15-88
4-3-87	REVISED ENERGY DISSIPATOR	671-4-3-87
1-9-87	MODIFIED NOTE ON ENERGY DISS.	532-1-9-87
11-3-86	ADDED NOTE TO ENERGY DISS.	599-12-1-86
11-1-84	ENERGY DISSIPATOR DETAILS	508-11-1-84
11-1-84	ADDED	
11-1-84	EXCAVATION DETAILS ADDED	
	TYPED A & B	
10-2-72	REVISED AND REDRAWN	508-10-2-72
DATE	REVISION	DATE FILM'D

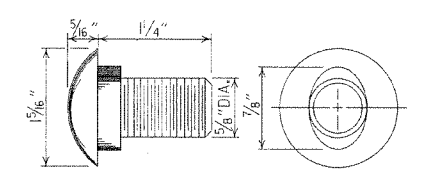
ARKANSAS STATE HIGHWAY COMMISSION

CONCRETE DITCH PAVING

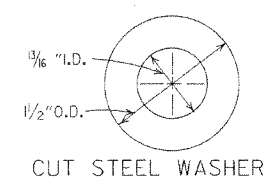
STANDARD DRAWING CDP-1



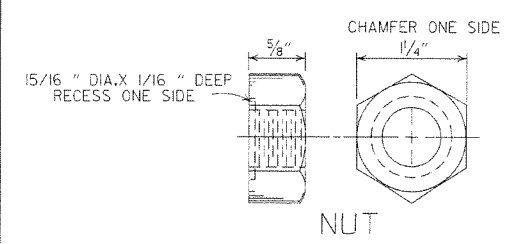
DETAILS OF W-BEAM GUARD RAIL
RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



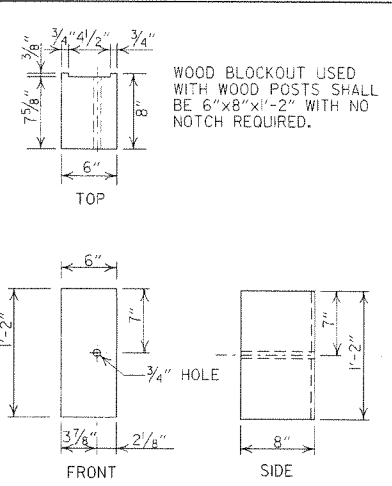
SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH



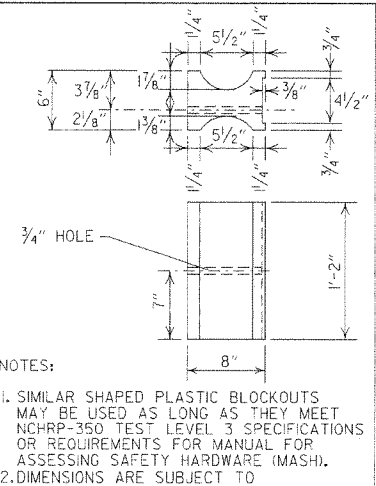
CUT STEEL WASHER



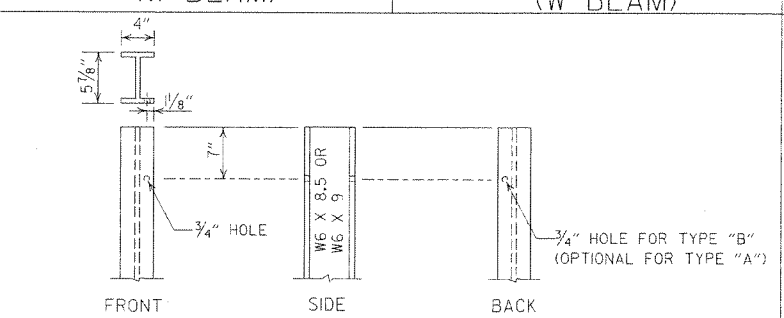
NUT



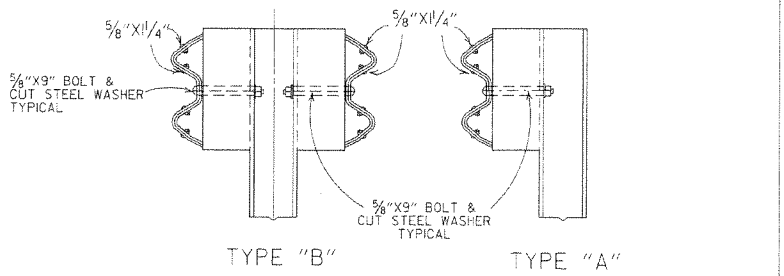
WOOD BLOCKOUT (W-BEAM)



PLASTIC BLOCKOUT (W-BEAM)



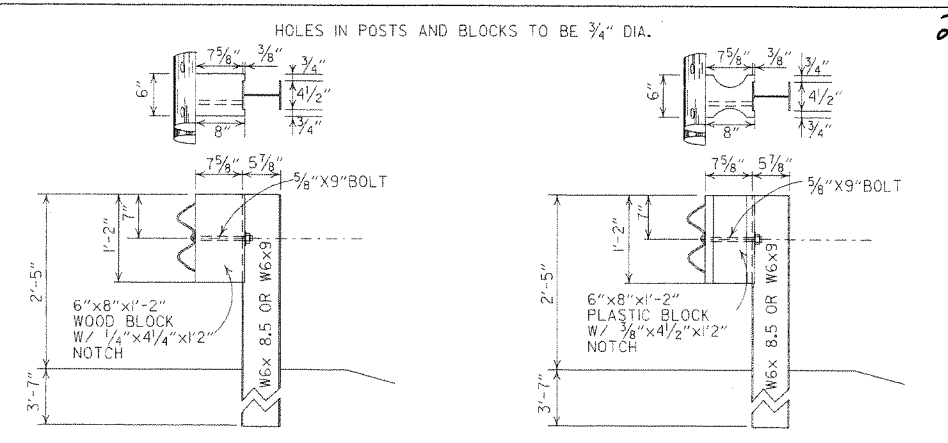
STEEL POST



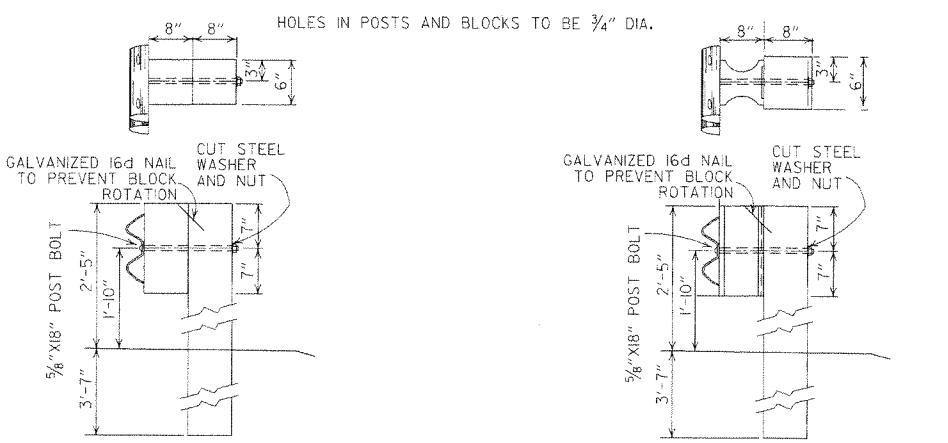
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)

-GENERAL NOTES-

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.
WHERE W-BEAM GUARD RAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.
W-BEAM GUARD RAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.
USE W-BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARD RAIL, W-BEAM GUARD RAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.
ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7 F (1400 F) OR NO. 1 350 F SOUTHERN PINE.
CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARD RAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS NCHRP-350 TEST LEVEL 3 SPECIFICATIONS OR REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARD RAIL.



WOOD BLOCKOUT CONNECTIONS
PLASTIC BLOCKOUT CONNECTIONS
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



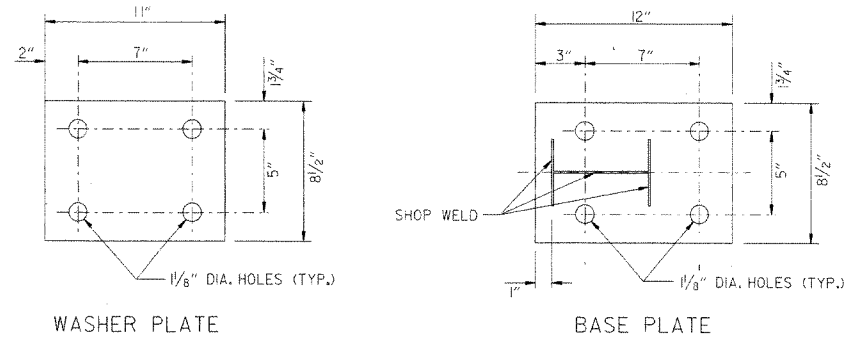
WOOD BLOCKOUT CONNECTIONS
PLASTIC BLOCKOUT CONNECTIONS
DETAILS OF WOOD LINE POST CONNECTIONS (W-BEAM)

DATE	REVISION	DATE FILM
7-4-10	RAISED HEIGHT OF GUARD RAIL 1"	
10-15-09	ADDED REFERENCE TO MASH	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & ON STEEL POST	
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS	
3-30-00	REMOVED GUARD RAIL AT BRIDGE ENDS	
1-2-00	ADDED PLASTIC BLOCKOUT	
8-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARD RAIL REPLACE, BEHIND CURB & DET. OF POST PLACE IN SOLID ROCK, & ADDED DETAILS OF STEEL LINE POST CONN. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES	
4-3-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS	
10-18-96	REVISED WOOD POST NOTE	
6-2-94	ADDED AL T. STEEL POST SIZE	
8-5-93	REVISED STEEL POST SIZE	8-5-93
10-1-92	REDRAWN & REVISED	10-1-92
8-15-91	REVISED WASHER NOTE	8-15-91
8-2-90	REV. GEN. NOTE & DEPTH OF ANC. POST IN ROCK	8-2-90
7-15-88	REVISED SECTION 3 & GENERAL NOTES	
3-4-88	REV. ANCHOR POST, ELEV. NOTES & POST IN ROCK	780-3-4-88
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87
10-9-87	REDRAWN & REVISED	802-10-9-87
	REVISION	

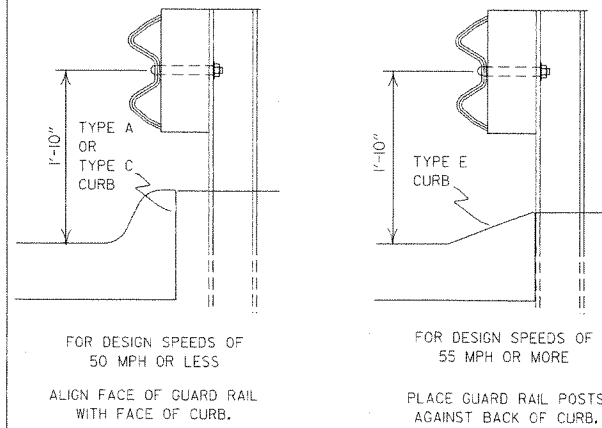
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

STANDARD DRAWING GR-8

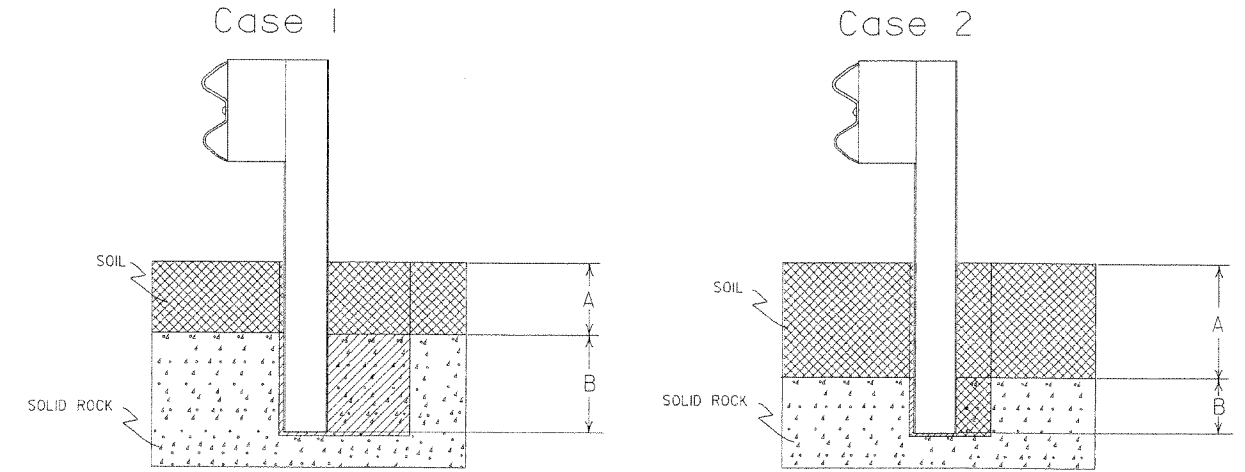


Note: Bolts, nuts, washers and plates shall be galvanized in accordance with Section 807 of the Standard Specifications.

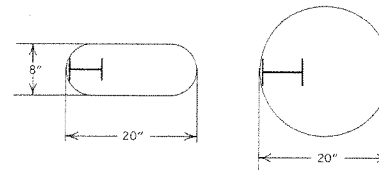


DETAIL OF GUARD RAIL PLACEMENT BEHIND CURB (W-BEAM)

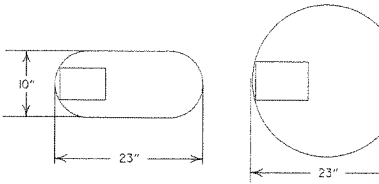
FOR DESIGN SPEEDS OF 50 MPH OR LESS ALL CURB FACES, AS SHOWN ON STD. DRWG. CG-1, MAY BE USED. FOR DESIGN SPEEDS OF 55 MPH OR MORE TYPE "E" CURB FACE SHALL BE USED.



Plan View Steel Posts
Either hole configuration acceptable



Plan View Wood Posts
Either hole configuration acceptable



Notes: For overlying soil depths (A) ranging from 0 to 18", the depth of required drilling (B) is equal to 24".

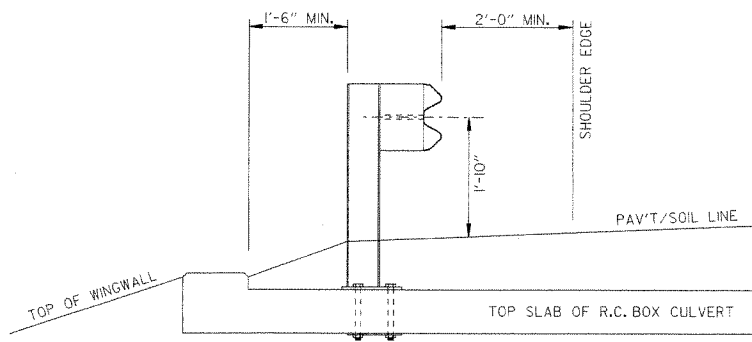
Zone A: Backfill according to Section 617.03(a).

Zone B: Backfill hole in 6" lifts with material meeting the requirements of Section 802.02(c) - Alternate gradation. Compact to 95% maximum dry density per ASTM D-698.

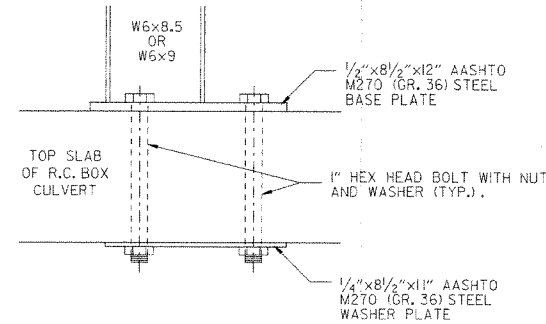
Notes: For overlying soil depths (A) ranging from 18" to 44", the depth of required drilling (B) is equal to either 12" or 44" minus the depth of soil whichever is less.

Zone A & B: Backfill according to Section 617.03(a).

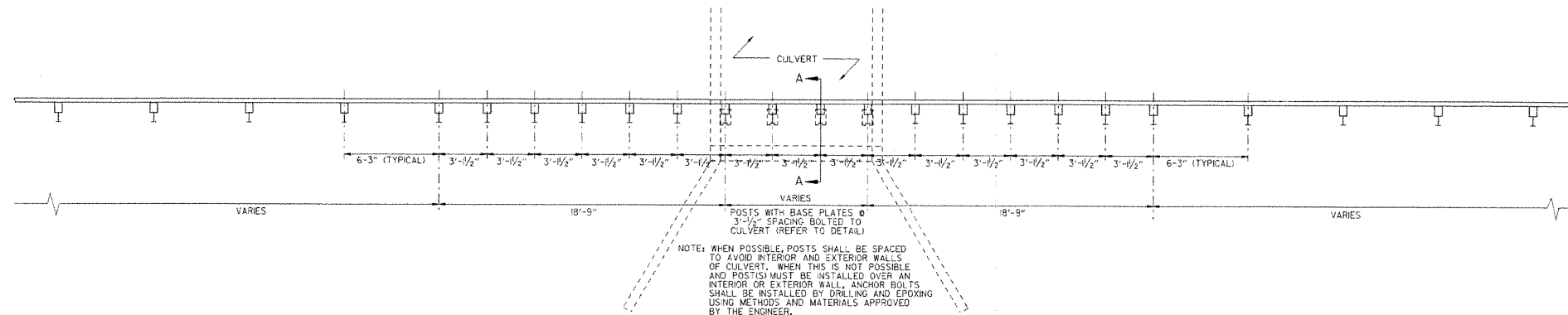
DETAIL OF POST PLACEMENT IN SOLID ROCK (W-BEAM)



SECTION A-A



DETAIL OF CONNECTION



PLAN LAYOUT OF TYPE A GUARD RAIL AT LOW-FILL CULVERTS

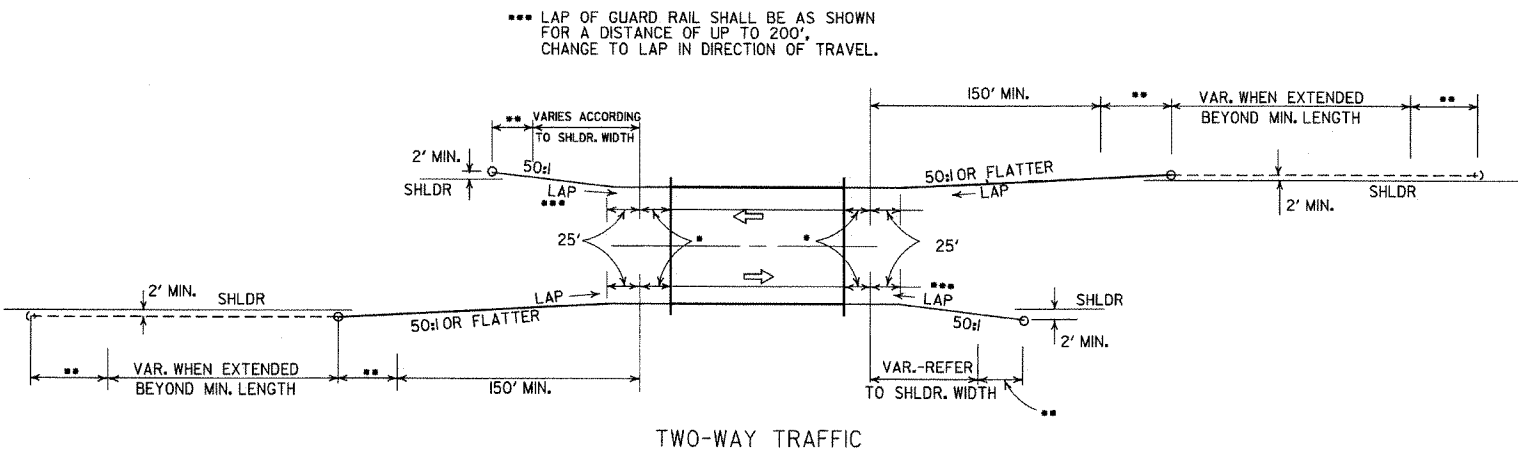
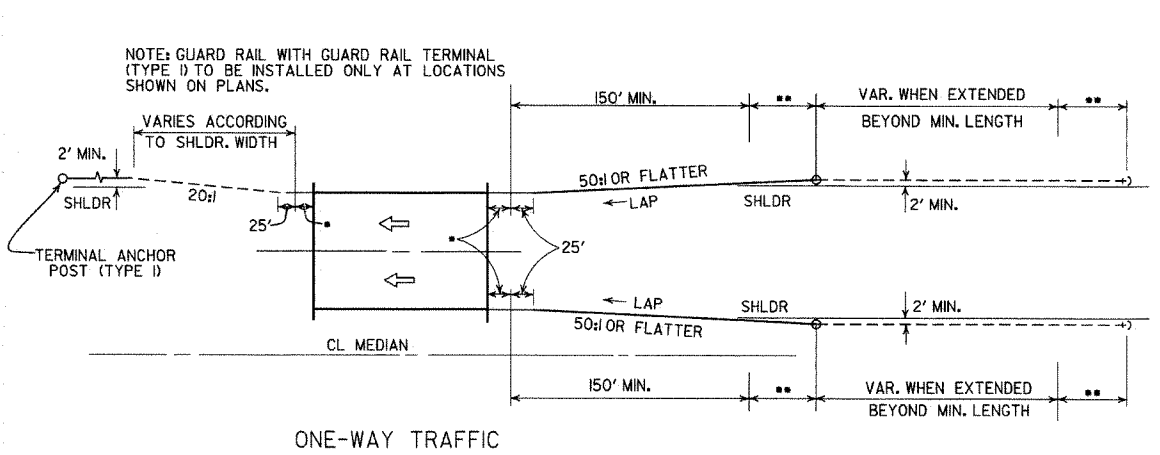
NOTE: THIS DETAIL IS TO BE USED ONLY WHEN THE COVER OVER THE CULVERT DOES NOT PERMIT FULL EMBEDMENT OF GUARD RAIL POSTS AS SHOWN ON STD. DWG. GR-2.

7-14-10	RAISED HEIGHT OF GUARD RAIL 1"	
4-12-07	REVISED DETAIL OF GUARD RAIL PLACEMENT BEHIND CURB	
11-10-05	ADDED GUARD RAIL PLACEMENT BEHIND CURB; REVISED DETAIL OF CONNECTION	
11-18-04	REVISED POST PLACEMENT IN ROCK & CULVERT CONNECTION DETAILS. ADDED DETAIL FOR GUARD RAIL PLACEMENT AT LOW-FILL CULVERTS	
3-30-00	REMOVED CONCRETE INSERT ANCHOR	
8-12-98	CHANGED STEEL SPACER BLOCK TO WOOD BLOCKOUT, ADD. DET. OF GUARD RAIL CONNECTION TO R.C. BOX CULVT. DELETED DET. OF STEEL LINE POST CONN. & ADDED DET. OF GUARD RAIL PLACE. BEHIND CURB & DET. OF POST PLACE. IN SOLID ROCK	
4-3-96	PLACED ARROWS AT CUT STEEL WASHERS	4-3-96
10-18-96	REV. ASTM REF. TO AASHTO	
1-22-95	ADDED OPTIONAL HOLES	
6-2-94	REVISED ALTERNATE POST SIZE	
8-5-93	REVISED STEEL POST SIZE	
10-1-92	REDRAWN & REVISED	10-1-92
8-2-90	DEL. WASHER ON ANCHOR ASSEMBLY	8-2-90
7-15-88	CONFORMED TO 1988 SPECS	
3-4-88	REVISED ANCHOR NOTE	
10-30-87	REVISED ANCHOR ASSEMBLY	7/2-10-30-87
10-30-87	REVISED PLACEMENT BEHIND CURB	5/47-10-30-87
10-9-87	REDRAWN & REVISED	803-10-9-87
DATE	REVISION	DATE FILED

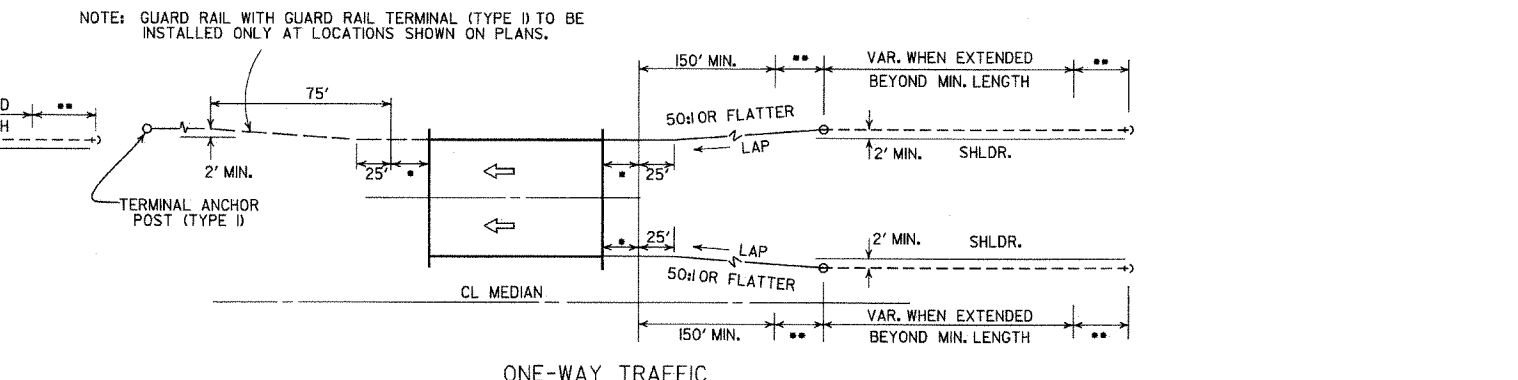
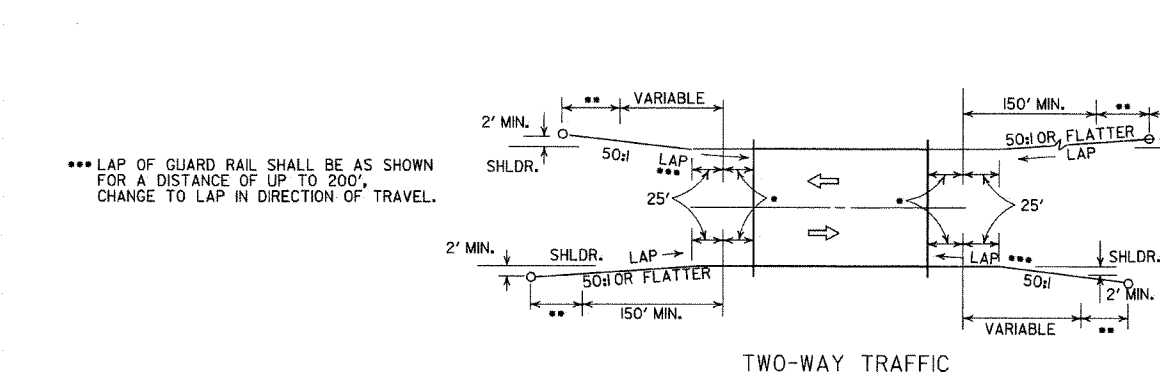
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

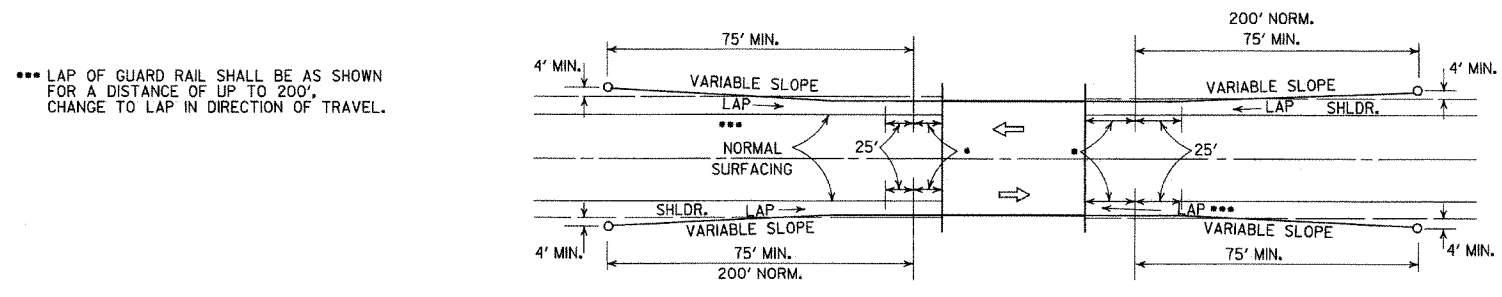
STANDARD DRAWING GR-8A



METHODS OF INSTALLATION OF GUARD RAIL AT LESS THAN FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)



METHOD OF INSTALLATION OF GUARD RAIL AT FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)

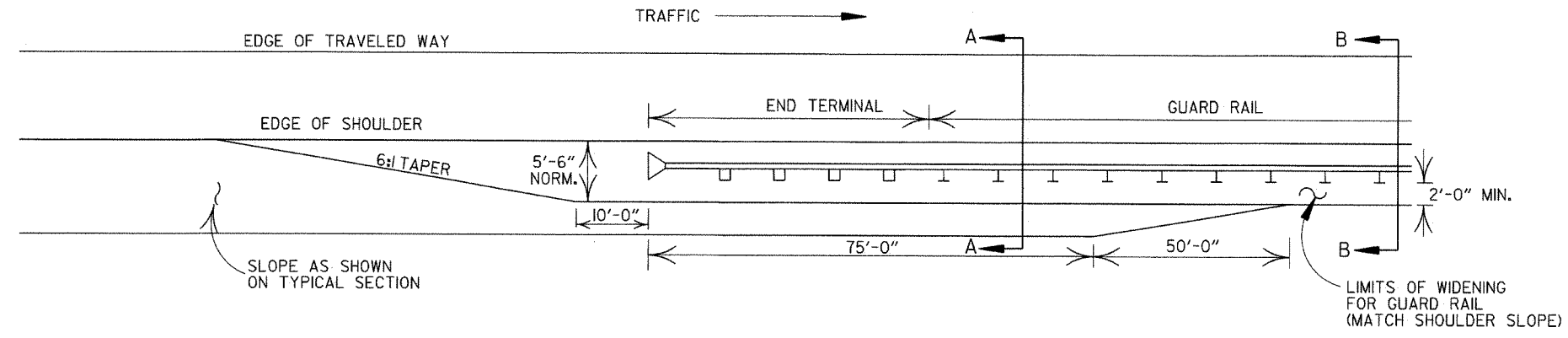


LEGEND

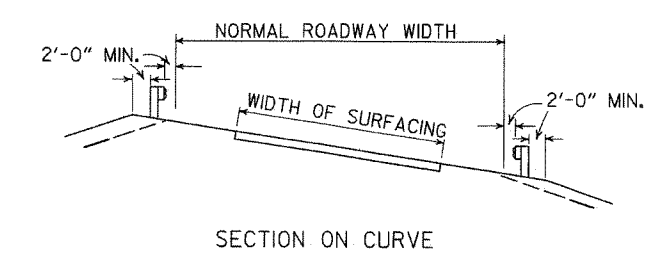
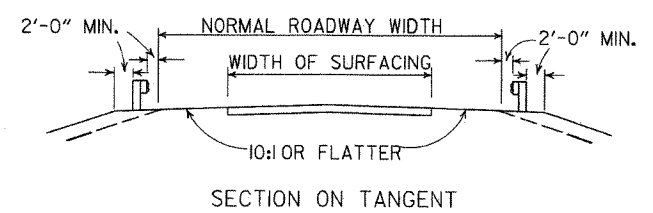
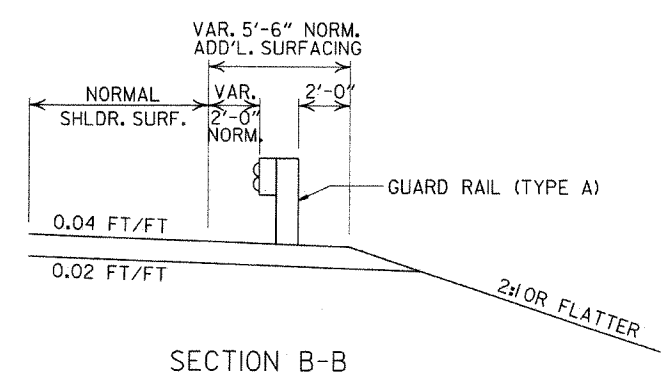
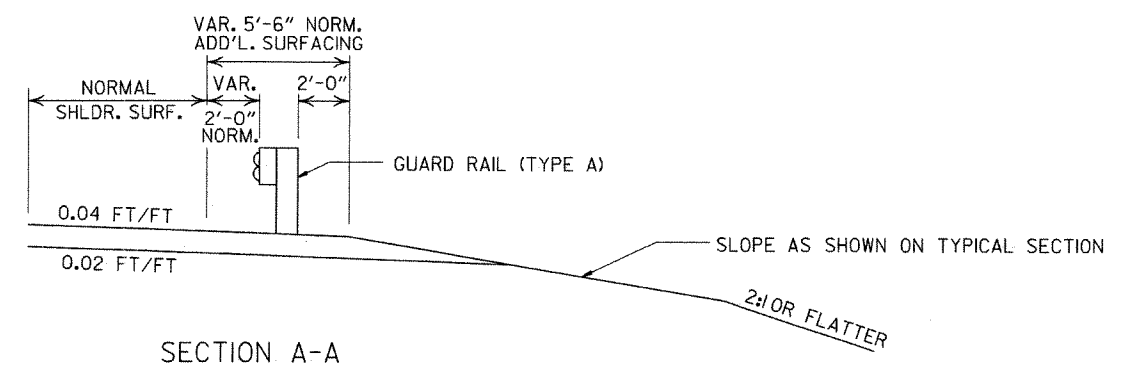
- THRIE BEAM GUARD RAIL TERMINAL
- GUARD RAIL TERMINAL (TYPE 2)

METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERMINAL (TYPE 1) (FULL SHOULDER WIDTH OR LESS BRIDGES)

ARKANSAS STATE HIGHWAY COMMISSION		
GUARD RAIL DETAILS		
STANDARD DRAWING GR-9		
4-17-08	REVISED LAYOUTS	
11-10-05	REMOVED GUARD RAIL NOTES AND DETAILS	
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERM. (TY. 1)	
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00
6-26-97	REVISED LAYOUT	
10-1-92	REDRAWN & REVISED	10-1-92
10-9-87	ADDED NOTE	
10-9-87	REDRAWN & REVISED	
DATE	REVISION	DATE FILM

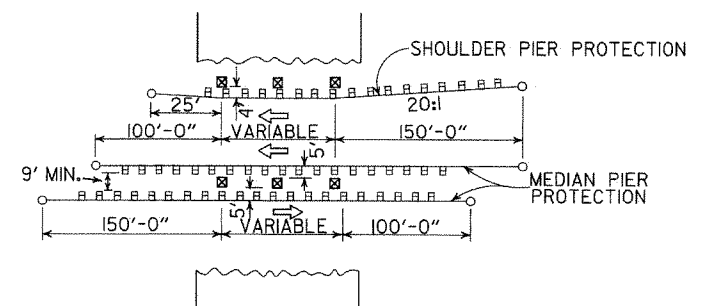


NOTE: NORMAL SECTION TO BE WIDENED APPROX. 5'-6" EACH SIDE TO SUPPORT GUARD RAIL.



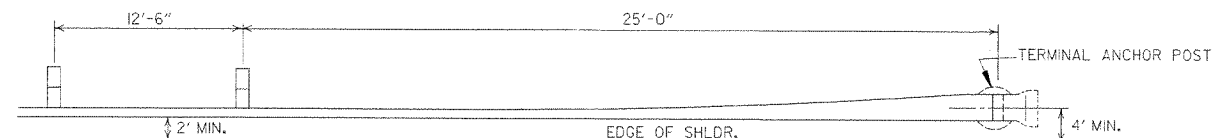
DETAILS OF WIDENING FOR GUARD RAIL

DETAILS SHOWING POSITION OF GUARD RAIL ON HIGHWAY

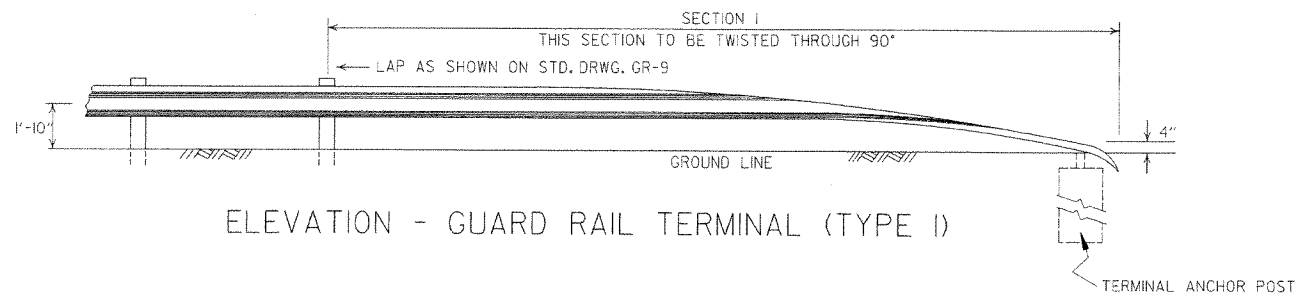


METHOD OF INSTALLATION OF GUARD RAIL AT FIXED OBSTACLE

ARKANSAS STATE HIGHWAY COMMISSION			
GUARD RAIL DETAILS			
STANDARD DRAWING GR-9A			
4-17-08	MINOR REVISION		
11-10-05	DRAWN		
DATE	REVISION	DATE	FILM

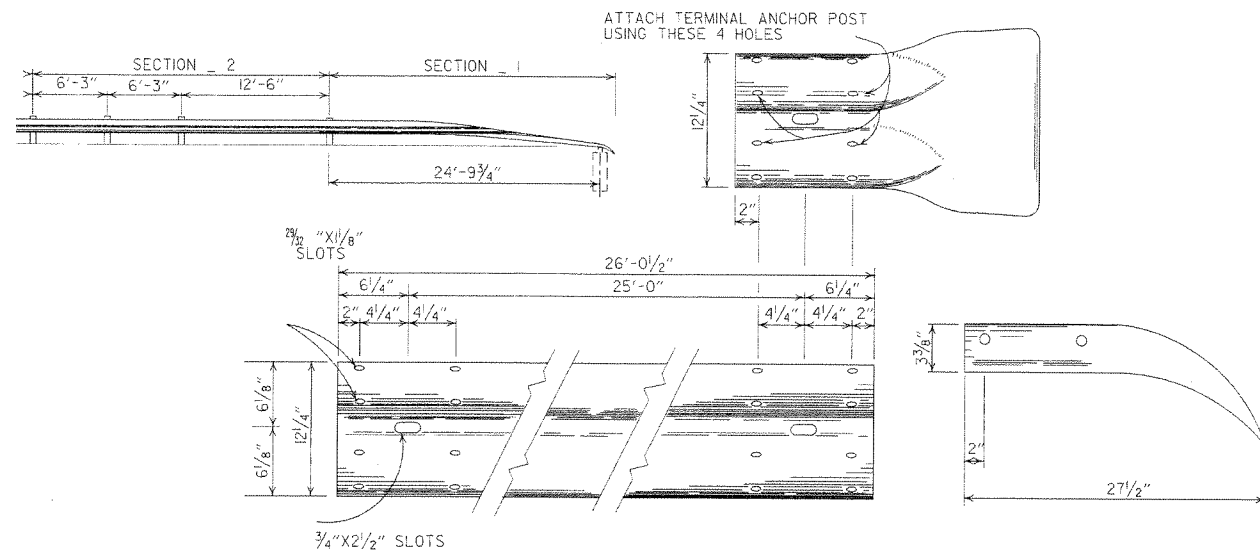


PLAN - GUARD RAIL TERMINAL (TYPE I)



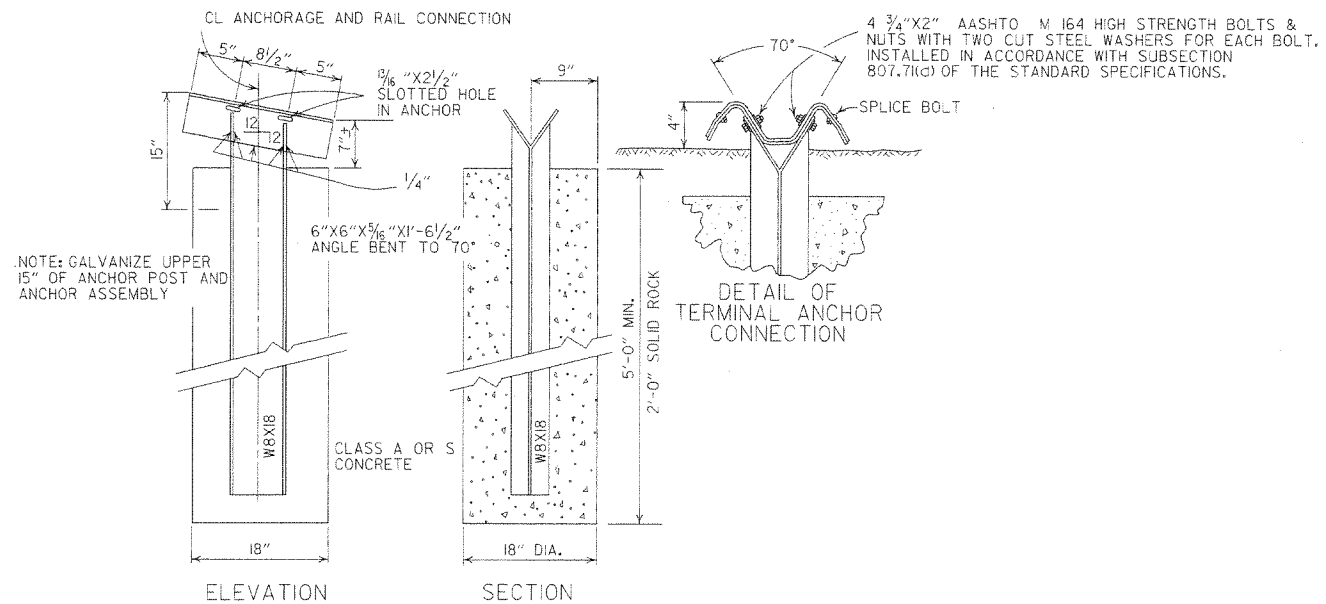
ELEVATION - GUARD RAIL TERMINAL (TYPE I)

NOTE:
SECTIONS 1 AND 2 OF GUARD RAIL TERMINAL
SHALL BE PAID FOR AT THE PRICE BID PER
LINEAR FOOT OF THE TYPE OF GUARD RAIL SPECIFIED.



SECTION 1

TERMINAL SECTION



ELEVATION SECTION

NOTE: RAIL MEMBERS MAY BE BOLTED TO ANGLE AT TERMINAL ANCHOR AND THE TWO ASSEMBLIES POSITIONED TO PROPER ALIGNMENT PRIOR TO PLACING CONCRETE AROUND 8 WF 17 POST IF CONTRACTOR SO DESIRES.

DETAIL OF TERMINAL ANCHOR POST (TYPE I)

			ARKANSAS STATE HIGHWAY COMMISSION
			GUARD RAIL DETAILS
7-14-10	RAISED HEIGHT OF GUARD RAIL 1"		STANDARD DRAWING GRT-1
6-26-97	REVISED LAP NOTE		
10-18-96	REVISED ASTM REF. TO AASHTO		
11-3-94	DIMENSION TERMINAL DETAIL		
11-11-92	ADDED NOTE FOR PAYMENT	11-11-92	
10-1-92	DRAWN & ISSUED	10-1-92	
DATE	REVISION	DATE FILM	

REINFORCED CONCRETE ARCH PIPE DIMENSIONS

EQUIV. DIA.	SPAN		RISE	
	AASHTO M 206	AHTD NOMINAL	AASHTO M 206	AHTD NOMINAL
INCHES	INCHES			
15	18	18	11	11
18	22	22	13 1/2	14
21	26	26	15 1/2	16
24	28 1/2	29	18	18
30	36 1/4	36	22 1/2	23
36	43 3/8	44	26 3/8	27
42	51 1/8	51	31 1/8	31
48	58 1/2	59	36	36
54	65	65	40	40
60	73	73	45	45
72	88	88	54	54
84	102	102	62	62
90	115	115	72	72
96	122	122	77 1/2	77
108	138	138	87 1/8	87
120	154	154	96 3/8	97
132	168 3/4	169	106 1/2	107

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M206.

REINFORCED CONCRETE HORIZONTAL ELLIPTICAL PIPE DIMENSIONS

EQUIV. DIA.	AASHTO M 207	
	SPAN	RISE
INCHES	INCHES	
18	23	14
24	30	19
27	34	22
30	38	24
33	42	27
36	45	29
39	49	32
42	53	34
48	60	38
54	68	43
60	76	48
66	83	53
72	91	58
78	98	63
84	106	68

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M207.

CONSTRUCTION SEQUENCE

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. PLACE AND COMPACT THE HAUNCH AREA UP TO THE MIDDLE OF THE PIPE.
5. COMPLETE BACKFILL ACCORDING TO SUBSECTION 606.03.(f)(1).

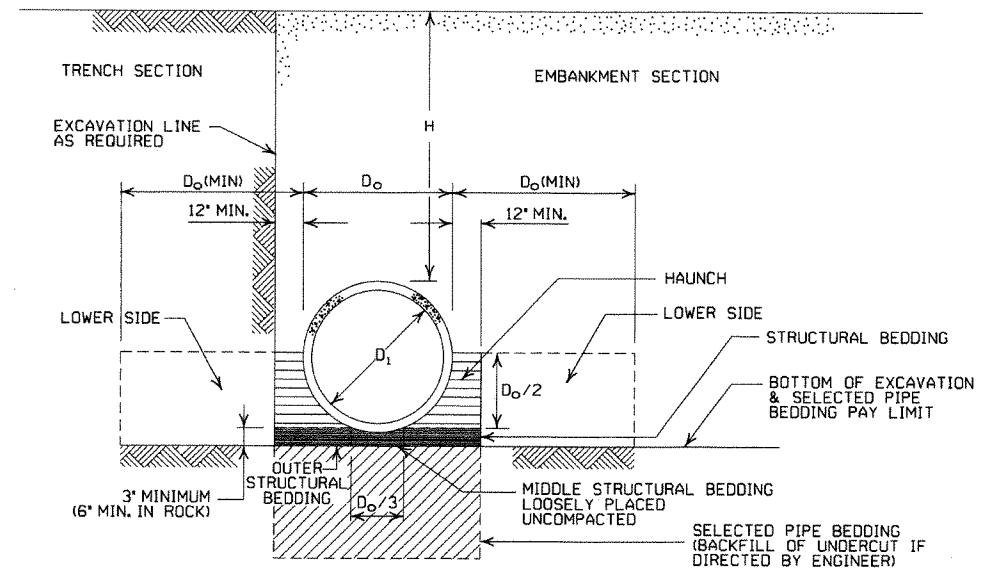
NOTE: HAUNCH AND STRUCTURAL BEDDING MATERIAL WILL NOT BE PAID FOR SEPARATELY, BUT COMPENSATION WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF CONCRETE PIPE.

- LEGEND -

- D₁ = NORMAL INSIDE DIAMETER OF PIPE
- D_o = OUTSIDE DIAMETER OF PIPE
- H = FILL COVER HEIGHT OVER PIPE (FEET)
- MIN. = MINIMUM
- [Symbol] = UNDISTURBED SOIL

INSTALLATION TYPE	MATERIAL REQUIREMENTS FOR HAUNCH AND STRUCTURAL BEDDING
TYPE 1	AGGREGATE BASE COURSE (CLASS 5 OR CLASS 7)
TYPE 2	SELECTED MATERIALS (CLASS SM-1, SM-2, OR SM-4) OR TYPE 1 INSTALLATION MATERIAL*
TYPE 3**	AASHTO CLASSIFICATION A-1 THRU A-6 SOIL OR TYPE 1 OR 2 INSTALLATION MATERIAL

- * SM-3 WILL NOT BE ALLOWED.
- ** MATERIALS SHALL NOT INCLUDE ORGANIC MATERIALS OR STONES LARGER THAN 3 INCHES.



EMBANKMENT AND TRENCH INSTALLATIONS

1. MATERIAL IN THE HAUNCH AND OUTER STRUCTURAL BEDDING SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.
2. FOR TRENCHES WITH WALLS OF NATURAL SOIL, THE DENSITY OF THE SOIL IN THE LOWER SIDE ZONE SHALL BE AS FIRM AS THE 95% DENSITY REQUIRED FOR THE HAUNCH. IF THE EXISTING SOIL DOES NOT MEET THIS CRITERIA, IT SHALL BE REMOVED AND RECOMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OF MATERIAL USED.
3. FOR EMBANKMENTS, THE MATERIAL IN THE LOWER SIDE ZONE SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.

GENERAL NOTES

1. CONCRETE PIPE CULVERT CONSTRUCTION SHALL CONFORM TO ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2003 EDITION), WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS, UNLESS OTHERWISE NOTED IN THE PLANS, SECTION AND SUBSECTION REFER TO THE STANDARD CONSTRUCTION SPECIFICATIONS.
2. CONCRETE PIPE CULVERT DESIGN SHALL CONFORM TO AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, FIFTH EDITION (2010) WITH 2010 INTERIMS.
3. ALL PIPE SHALL CONFORM TO SECTION 606. CIRCULAR R.C. PIPE CULVERTS SHALL CONFORM TO AASHTO M170, R.C. ARCH PIPE CULVERTS SHALL CONFORM TO AASHTO M206 AND HORIZONTAL ELLIPTICAL PIPE CULVERTS SHALL CONFORM TO AASHTO M207.
4. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
5. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
6. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE. REFER TO STD. DWG. FES-2 FOR MINIMUM CLEARANCE WHERE FLARED END SECTIONS ARE USED.
7. IMPERVIOUS MATERIAL SHOULD BE PLACED AS DIRECTED BY THE ENGINEER AT THE ENDS OF THE CULVERT TO PREVENT LOSS OF STRUCTURAL BEDDING WHEN PERVIOUS MATERIAL IS USED FOR STRUCTURAL BEDDING AND/OR BACKFILL.
8. NOT MORE THAN ONE LIFTING HOLE MAY BE PROVIDED IN CONCRETE PIPE TO FACILITATE HANDLING. HOLE MAY BE CAST IN PLACE, CUT INTO THE FRESH CONCRETE AFTER FORMS ARE REMOVED, OR DRILLED. THE HOLE SHALL NOT BE MORE THAN TWO INCHES IN DIAMETER OR TWO INCHES SQUARE. CUTTING OR DISPLACEMENT OF REINFORCEMENT WILL NOT BE PERMITTED. SPALLED AREAS AROUND THE HOLE SHALL BE REPAIRED IN A WORKMANLIKE MANNER. LIFTING HOLE SHALL BE FILLED WITH MORTAR, CONCRETE, OR OTHER METHOD AS APPROVED BY THE ENGINEER.
9. WHEN DIRECTED BY THE ENGINEER, UNSUITABLE MATERIAL THAT IS ENCOUNTERED AT THE BOTTOM OF THE EXCAVATED TRENCH (BELOW THE AREA IDENTIFIED AS "STRUCTURAL BEDDING" ABOVE) WILL BE EXCAVATED AND REPLACED WITH SELECTED PIPE BEDDING. THE QUANTITY OF MATERIAL REQUIRED TO BACKFILL THE UNDERCUT AREA UP TO THE SELECTED PIPE BEDDING PAY LIMIT DESIGNATED ABOVE WILL BE MEASURED AND PAID FOR AS "SELECTED PIPE BEDDING."
10. WHEN THE EXISTING MATERIAL EXCAVATED FOR THE PIPE TRENCH IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING THE PIPE (ABOVE THE AREA IDENTIFIED ABOVE AS THE HAUNCH), BORROW MATERIAL OR MATERIAL FROM THE ROADWAY EXCAVATION WILL BE USED TO BACKFILL THE PIPE. IF SUITABLE MATERIAL IS NOT AVAILABLE, THE ENGINEER MAY AUTHORIZE THE USE OF "SELECTED PIPE BACKFILL."

MINIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE			
	CLASS III		CLASS IV	CLASS V
PIPE ID (IN.)	FEET			
12-15	2	2.5	2	1
18-24	2.5	3	2	1
27-33	3	4	2	1
36-42	3.5	5	2	1
48	4.5	5.5	2	1
54-60	5	7	2	1
66-78	6	8	2	1
84-108	7.5	8	2	1

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE		
	CLASS III	CLASS IV	CLASS V
TYPE 1	21	32	50
TYPE 2	16	25	39
TYPE 3	12	20	30

NOTE: IF FILL HEIGHT EXCEEDS 50 FEET, A SPECIAL DESIGN CONCRETE PIPE WILL BE REQUIRED USING TYPE 1 INSTALLATION.

MINIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
TYPE 2 OR TYPE 3	FEET	
	2.5	1.5

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
TYPE 2	13	21
TYPE 3	10	16

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

DATE	REVISION	DATE FILMED
12-15-11	REVISED FOR LRFD DESIGN SPECIFICATIONS	
5-18-00	REVISED TYPE 3 BEDDING & ADDED NOTE	
3-30-00	REVISED INSTALLATIONS	
11-06-97	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

CONCRETE PIPE CULVERT
FILL HEIGHTS & BEDDING

STANDARD DRAWING PCC-1

CORRUGATED STEEL PIPE (ROUND)

PIPE DIAMETER (INCHES)	① MINIMUM COVER TOP OF PIPE TO TOP OF GROUND "H" (FEET)	MAX. FILL HEIGHT "H" ABOVE TOP OF PIPE (FEET)				
		METAL THICKNESS (INCHES)				
		0.064	0.079	0.109	0.138	0.168
2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED, WELDED, OR HELICAL LOCK-SEAM						
12	1	84	91			
15	1	67	73			
18	1	56	61			
24	1	42	46	59		
30	2	34	36	47		
36	2		30	39	41	73
42	2		43	67	70	73
48	2		37	58	61	64
② 3 INCH BY 1 INCH OR 5 INCH BY 1 INCH CORRUGATION RIVETED, WELDED, BOLTED, OR HELICAL LOCK-SEAM						
36	1	48	60	88	118	
42	1	41	51	72	90	102
48	1	36	45	64	77	85
54	2	32	40	59	71	79
60	2	29	36	53	64	71
66	2	26	33	47	58	64
72	2	24	30	44	53	59
78	2		28	41	49	54
84	2		26	38	45	51
90	2		24	35	43	45
96	2		22	33	40	44
102	2			31	38	42
108	2			30	35	39
114	2			28	34	37
120	2			27	32	35

CONSTRUCTION SEQUENCE

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. COMPLETE STRUCTURAL BACKFILL OPERATION BY WORKING FROM SIDE TO SIDE OF THE PIPE. THE SIDE TO SIDE STRUCTURAL BACKFILL DIFFERENTIAL SHALL NOT EXCEED 24 INCHES OR 1/3 THE SIZE OF THE PIPE, WHICHEVER IS LESS.

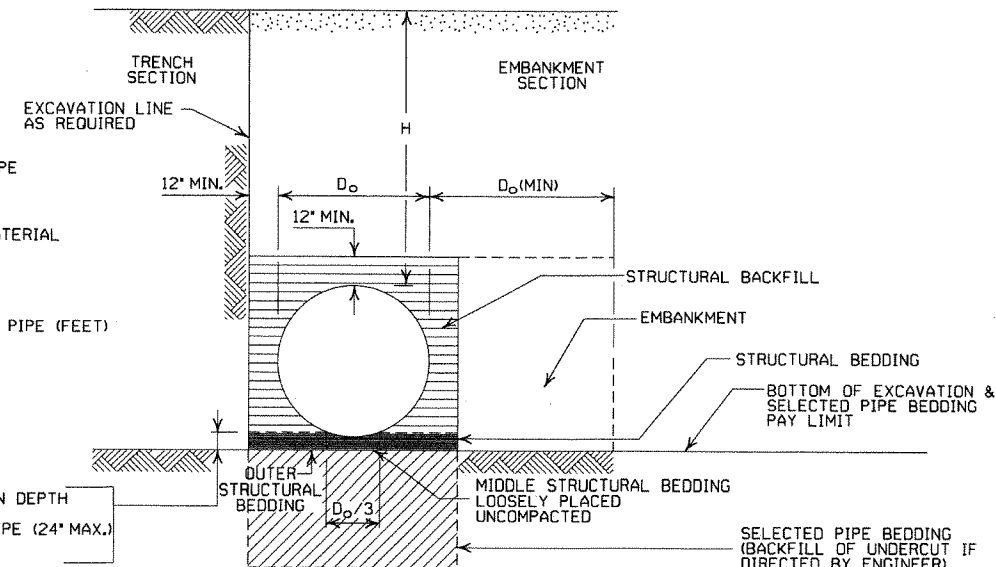
NOTE: STRUCTURAL BACKFILL AND STRUCTURAL BEDDING MATERIAL WILL NOT BE PAID FOR SEPARATELY, BUT COMPENSATION WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF METAL PIPE.

INSTALLATION TYPE	MATERIAL REQUIREMENTS FOR STRUCTURAL BACKFILL AND STRUCTURAL BEDDING
TYPE 1	AGGREGATE BASE COURSE (CLASS 4, 5, 6, OR 7)
TYPE 2	SELECTED MATERIALS (CLASS SM-1, SM-2, OR SM-4) OR TYPE 1 INSTALLATION MATERIAL ③

③ SM-3 WILL NOT BE ALLOWED.

- LEGEND -

- D_o = OUTSIDE DIAMETER OF PIPE
- MAX. = MAXIMUM
- MIN. = MINIMUM
- [Symbol] = STRUCTURAL BACKFILL MATERIAL
- [Symbol] = UNDISTURBED SOIL
- EQUIV. DIA. = EQUIVALENT DIAMETER
- H = FILL COVER HEIGHT OVER PIPE (FEET)



EMBANKMENT AND TRENCH INSTALLATIONS

1. STRUCTURAL BACKFILL, EMBANKMENT, AND OUTER STRUCTURAL BEDDING MATERIAL SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.
2. INSTALLATION TYPE 1 OR 2 MAY BE USED FOR CORRUGATED STEEL OR ALUMINUM PIPE (ROUND).
3. INSTALLATION TYPE 1 SHALL BE USED FOR CORRUGATED STEEL OR ALUMINUM PIPE ARCHES WITH 2 3/8" x 1/2" CORRUGATION.
4. INSTALLATION TYPE 1 OR 2 MAY BE USED FOR CORRUGATED STEEL OR ALUMINUM PIPE ARCHES WITH 3" x 1" OR 5" x 1" CORRUGATION.

GENERAL NOTES

1. METAL PIPE CULVERT CONSTRUCTION SHALL CONFORM TO ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2003 EDITION), WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS, UNLESS OTHERWISE NOTED IN THE PLANS, SECTION AND SUBSECTION REFER TO THE STANDARD CONSTRUCTION SPECIFICATIONS.
2. METAL PIPE CULVERT DESIGN SHALL CONFORM TO AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, FIFTH EDITION (2010) WITH 2010 INTERIMS.
3. METAL PIPE CULVERT MATERIALS AND INSTALLATIONS SHALL CONFORM TO SECTION 606 AND JOB SPECIAL PROVISION "METAL PIPE".
4. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
5. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
6. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE. REFER TO STD. DWG. FES-2 FOR MINIMUM CLEARANCE WHERE FLARED END SECTIONS ARE USED.
7. IMPERVIOUS MATERIAL SHOULD BE PLACED AS DIRECTED BY THE ENGINEER AT THE ENDS OF THE CULVERT TO PREVENT LOSS OF STRUCTURAL BEDDING WHEN PERVIOUS MATERIAL IS USED FOR STRUCTURAL BEDDING AND/OR BACKFILL.
8. WHEN DIRECTED BY THE ENGINEER, UNSUITABLE MATERIAL THAT IS ENCOUNTERED AT THE BOTTOM OF THE EXCAVATED TRENCH (BELOW THE AREA IDENTIFIED AS "STRUCTURAL BEDDING" ABOVE) WILL BE EXCAVATED AND REPLACED WITH SELECTED PIPE BEDDING. THE QUANTITY OF MATERIAL REQUIRED TO BACKFILL THE UNDERCUT AREA UP TO THE SELECTED PIPE BEDDING PAY LIMIT DESIGNATED ABOVE WILL BE MEASURED AND PAID FOR AS "SELECTED PIPE BEDDING."
9. WHEN THE EXISTING MATERIAL EXCAVATED FOR THE PIPE TRENCH IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING THE PIPE (ABOVE THE AREA IDENTIFIED AS STRUCTURAL BACKFILL), BORROW MATERIAL OR MATERIAL FROM THE ROADWAY EXCAVATION WILL BE USED TO BACKFILL THE PIPE. IF SUITABLE MATERIAL IS NOT AVAILABLE, THE ENGINEER MAY AUTHORIZE THE USE OF "SELECTED PIPE BACKFILL."

CORRUGATED ALUMINUM PIPE (ROUND)

PIPE DIAMETER (INCHES)	① MINIMUM COVER TOP OF PIPE TO TOP OF GROUND "H" (FEET)	MAX. FILL HEIGHT "H" ABOVE TOP OF PIPE (FEET)				
		METAL THICKNESS IN INCHES				
		0.060	0.075	0.105	0.135	0.164
2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED OR HELICAL LOCK-SEAM						
12	1	45	45	52		
18	2	30	30	39		
24	2	22	22	31	41	34
30	2		18	26	27	28
36	2.5		15	26	27	28
42	2			43	43	44
48	2			40	41	43
54	2			35	37	38
60	2				33	34
66	2					31
72	2					29

EQUIVALENT METAL THICKNESSES AND GAUGES

METAL THICKNESS IN INCHES			GAUGE NUMBER
STEEL			
ZINC COATED	UNCOATED	ALUMINUM	
0.064	0.0598	0.060	16
0.079	0.0747	0.075	14
0.109	0.1046	0.105	12
0.138	0.1345	0.135	10
0.168	0.1644	0.164	8

CORRUGATED METAL PIPE ARCHES

EQUIV. DIA. (INCHES)	PIPE DIMENSION SPAN X RISE (INCHES)	MINIMUM CORNER RADIUS (INCHES)	STEEL				ALUMINUM				
			MIN. THICKNESS REQUIRED INCHES	① MIN. HEIGHT OF FILL, "H" (FT.)		MIN. THICKNESS REQUIRED INCHES	① MIN. HEIGHT OF FILL, "H" (FT.)				
				INSTALLATION TYPE 1	INSTALLATION TYPE 2		INSTALLATION TYPE 1	INSTALLATION TYPE 2			
			2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED, WELDED, OR HELICAL LOCK-SEAM						2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED OR HELICAL LOCK-SEAM		
15	17x13	3	0.064	2	15	0.060	2	15			
18	21x15	3	0.064	2	15	0.060	2	15			
21	24x18	3	0.064	2.25	15	0.060	2.25	15			
24	28x20	3	0.064	2.5	15	0.075	2.5	15			
30	35x24	3	0.079	3	12	0.075	3	12			
36	42x29	3 1/2	0.079	3	12	0.105	3	12			
42	49x33	4	0.079	3	12	0.105	3	12			
48	57x38	5	0.109	3	13	0.135	3	13			
54	64x43	6	0.109	3	14	0.135	3	14			
60	71x47	7	0.138	3	15	0.164	3	15			
66	77x52	8	0.168	3	15						
72	83x57	9	0.168	3	15						
② 3 INCH BY 1 INCH OR 5 INCH BY 1 INCH CORRUGATION RIVETED, WELDED, OR HELICAL LOCK-SEAM											
			INSTALLATION TYPE 1				INSTALLATION TYPE 2				
36	40x31	5	0.079	3	2	12	15	15			
42	46x36	6	0.079	3	2	13	15	15			
48	53x41	7	0.079	3	2	13	15	15			
54	60x46	8	0.079	3	2	13	15	15			
60	66x51	9	0.079	3	2	13	15	15			
66	73x55	12	0.079	3	2	15	15	15			
72	81x59	14	0.079	3	2	15	15	15			
78	87x63	14	0.079	3	2	15	15	15			
84	95x67	16	0.109	3	2	15	15	15			
90	103x71	16	0.109	3	2	15	15	15			
96	112x75	18	0.109	3	2	15	15	15			
102	117x79	18	0.109	3	2	15	15	15			
108	128x83	18	0.138	3	2	15	15	15			

① FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM 12" OF PAVEMENT AND/OR BASE.

② WHERE THE STANDARD 2 2/3" x 1/2" CORRUGATION AND GAUGE IS SPECIFIED FOR A GIVEN DIAMETER, A PIPE OF THE SAME DIAMETER WITH A 3" x 1" OR 5" x 1" CORRUGATION MAY BE SUBSTITUTED, PROVIDING IT IS GAUGED FOR A FILL HEIGHT CONDITION EQUAL TO OR GREATER THAN THE MAXIMUM FILL HEIGHT CONDITION FOR THE SPECIFIED GAUGE AND CORRUGATION.

DATE	REVISION	DATE FILMED
12-15-11	REVISED FOR LRFD DESIGN SPECS	
3-30-00	REVISED INSTALLATIONS	
11-06-97	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

METAL PIPE CULVERT FILL HEIGHTS & BEDDING

STANDARD DRAWING PCM-1

ADVANCE DISTANCES (XXXX)


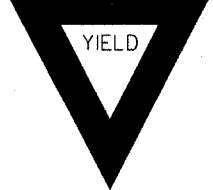
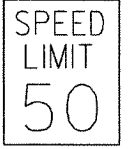
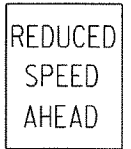





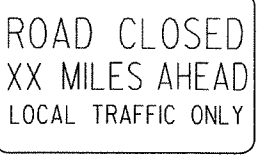
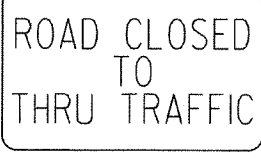
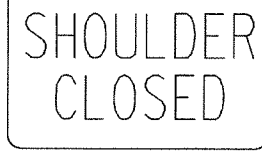
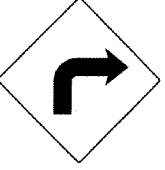

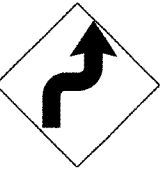



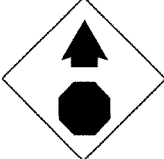
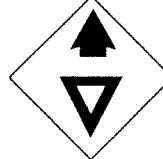
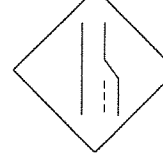

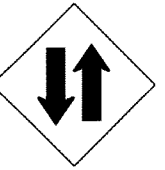

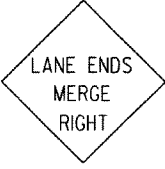


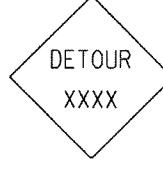


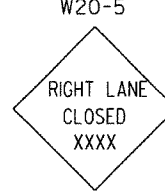






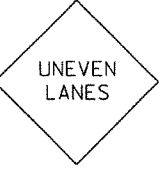
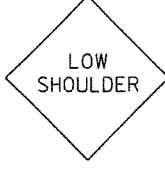
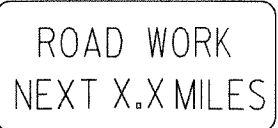
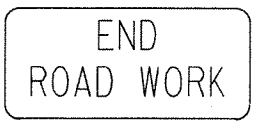
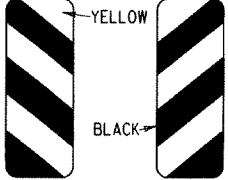


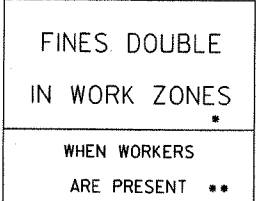
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1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

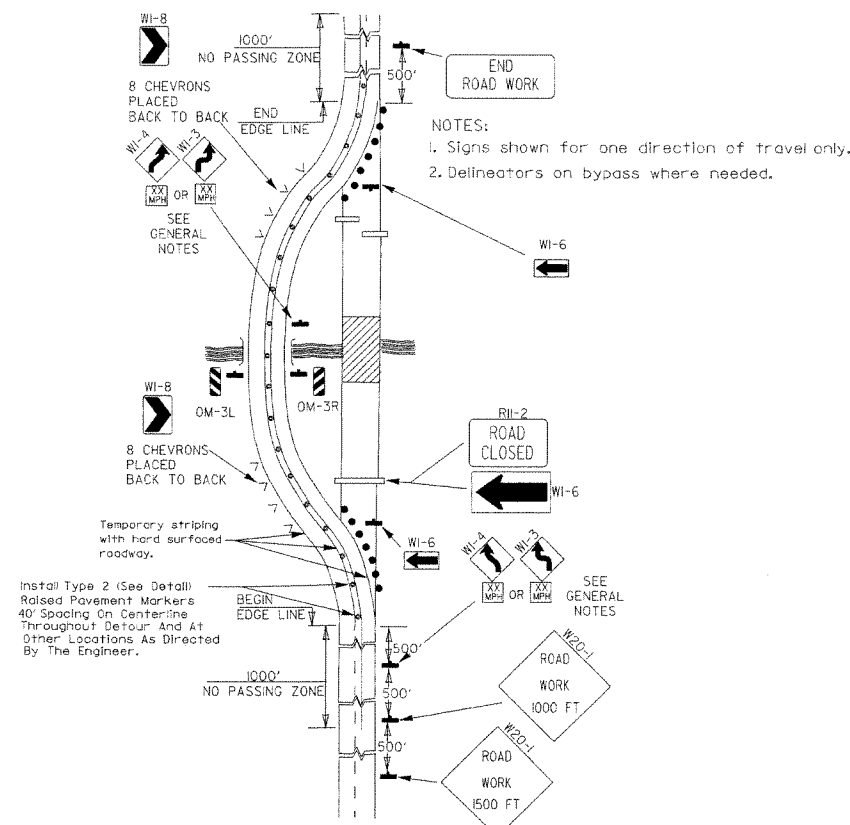
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

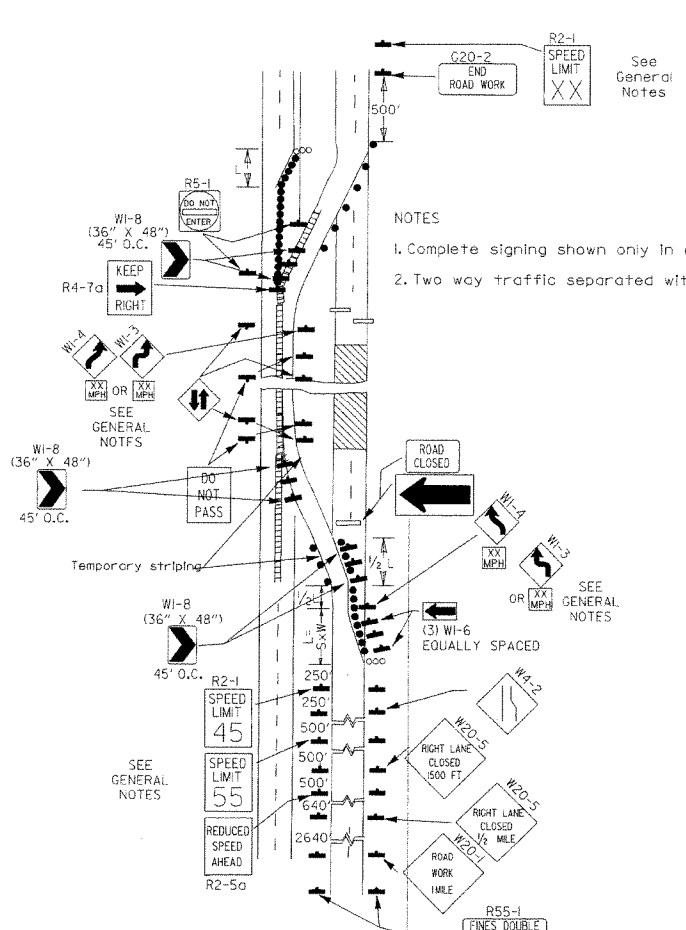
* NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
1-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
1-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

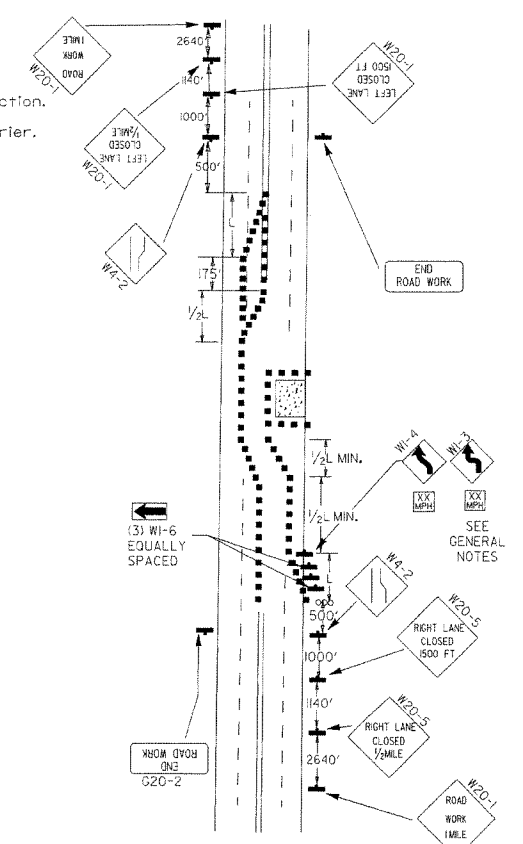
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>
<p>W20-3</p>  <p>STD. 48"x48"</p>	<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>
<p>WI-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>	<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>
<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>* USE 6" C LETTERS ** USE 4" D LETTERS</p>				



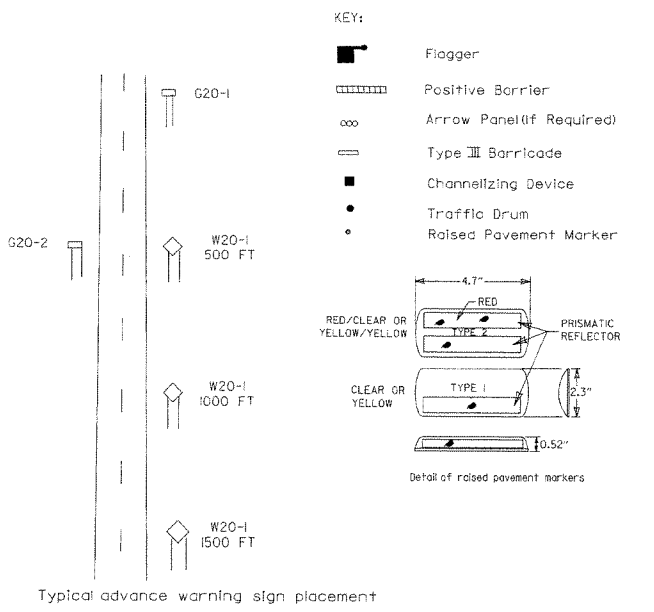
(A) Typical application of traffic control devices on a 2-lane highway where the entire roadway is closed and a bypass detour is provided.



(B) Typical application - 4-lane divided roadway where one roadway is closed.



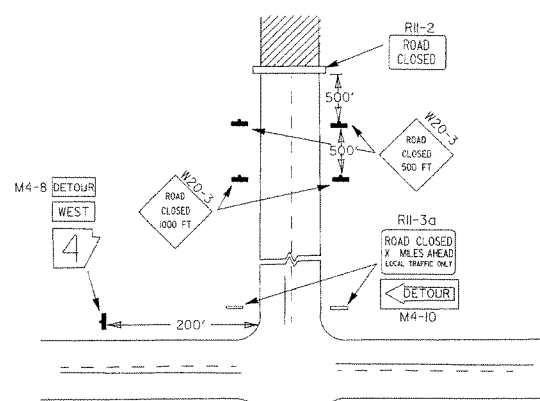
(C) Typical application - 4-lane undivided roadway where half of the roadway is closed.



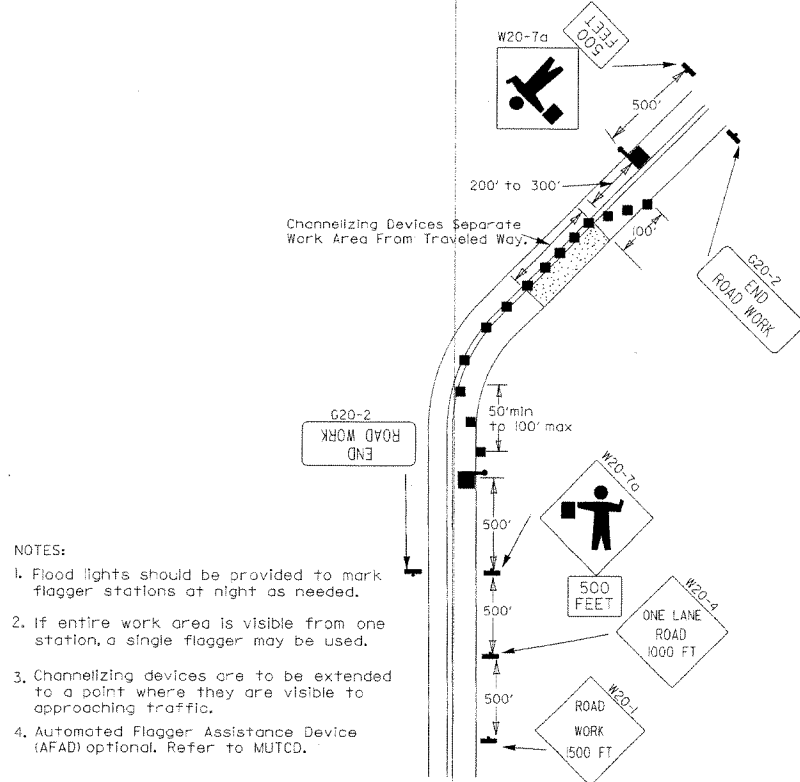
Taper formulae:
 $L = S \times W$ for speeds of 45mph or more.
 $L = \frac{WS^2}{60}$ for speeds of 40mph or less.
 Where:
 L = Minimum length of taper.
 S = Numerical value of posted speed limit prior to work or 85th percentile speed.
 W = Width of offset.

GENERAL NOTES:

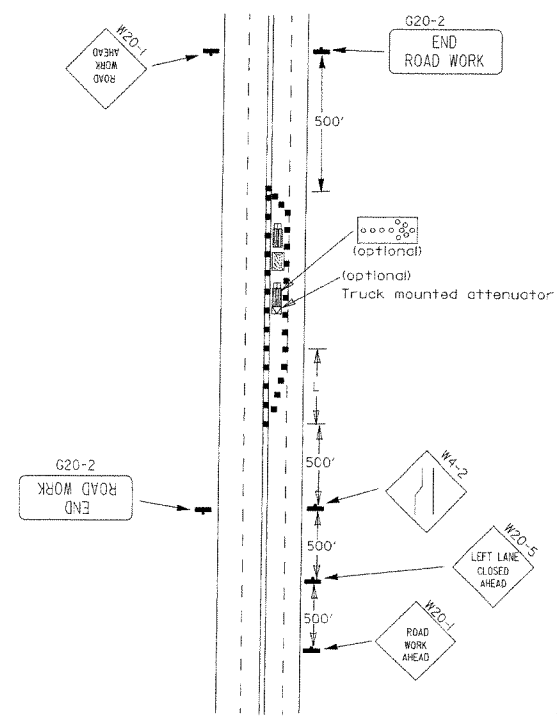
- Advisory speed posted on W1-3 or W1-4 curve warning signs to be determined at site. Use W1-4 when speed is greater than 30mph and W1-3 when 30mph or less.
- When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(45) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(45) shall be installed to match original speed limit.
- When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(45) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(45) shall be installed to match original speed limit.
- The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit, or as directed by the Engineer.
- Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
- Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
- Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.



(D) Typical application - roadway closed beyond detour point.



(E) Typical application of traffic control devices on 2-lane highway where one lane is closed and flagging is provided.

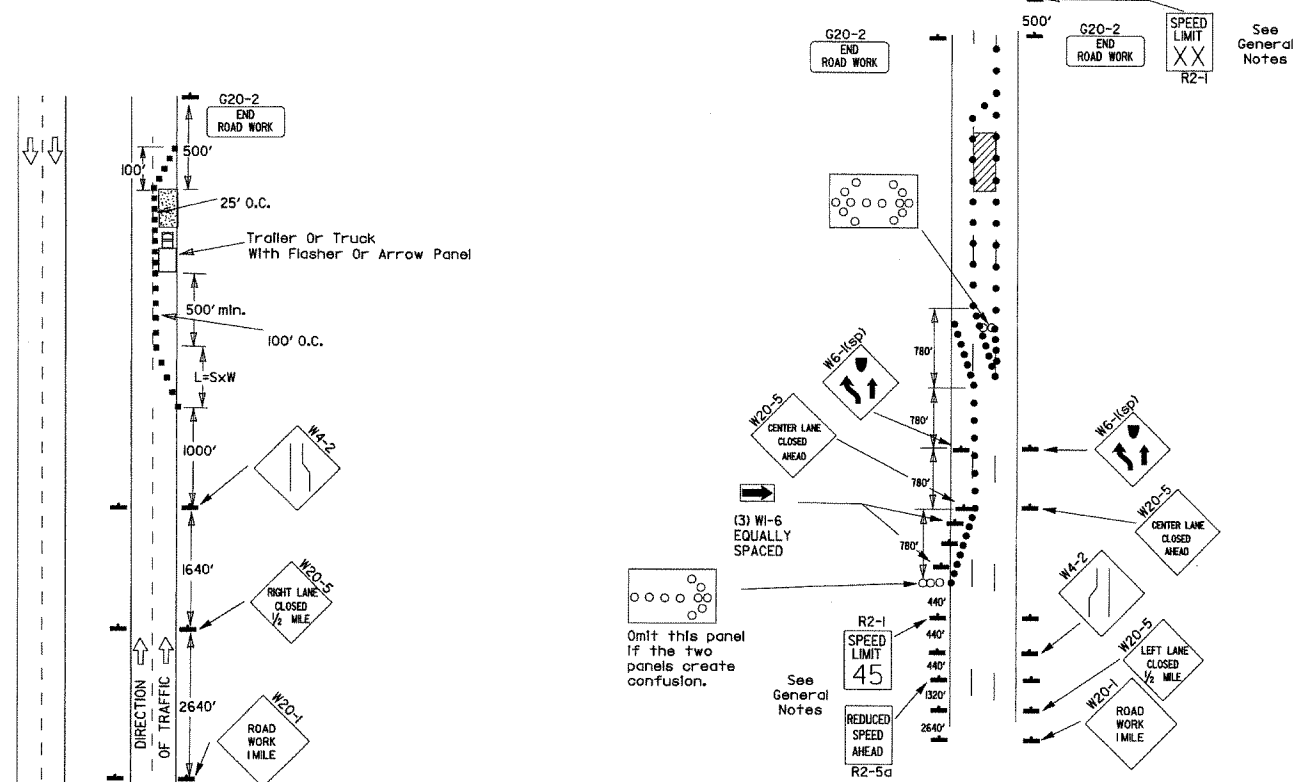


(F) Typical application - 4-lane undivided roadway with inside lane closed.

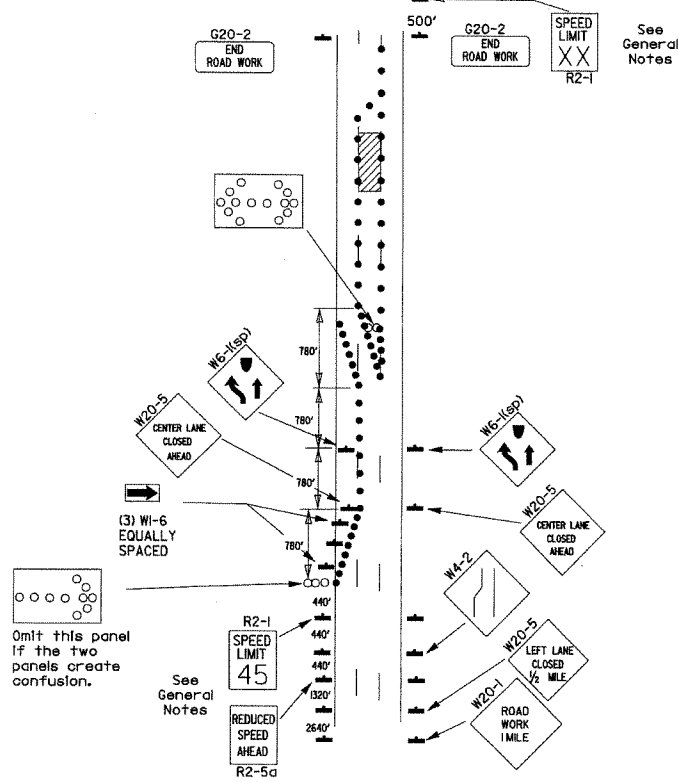
DATE	REVISION	FILMED
3-8-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-5-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
 STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION
 STANDARD DRAWING TC-2

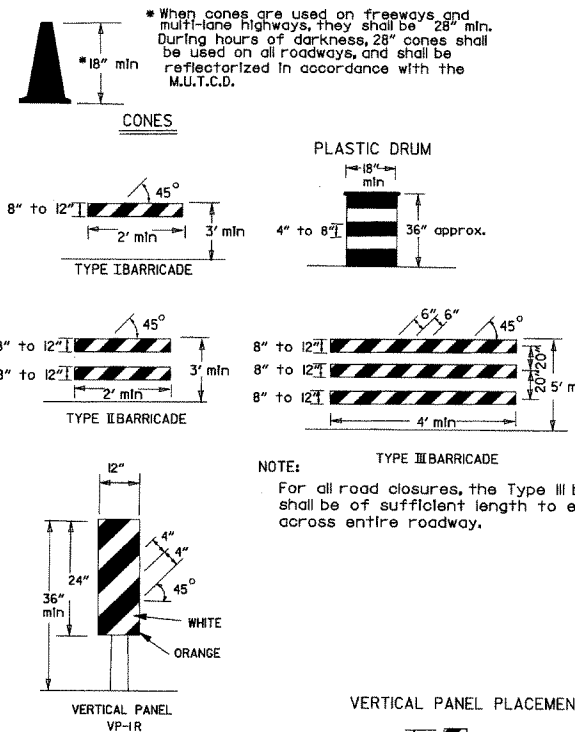
Channelizing devices



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



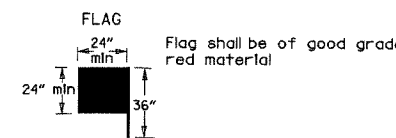
(B) Typical application - 3-lane oneway roadway where center lane is closed.



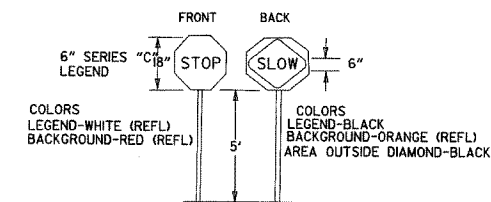
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-II
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-lane vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

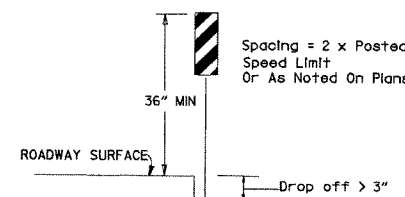
* When shown on the plans concrete barrier will be used. When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



STOP SLOW PADDLE

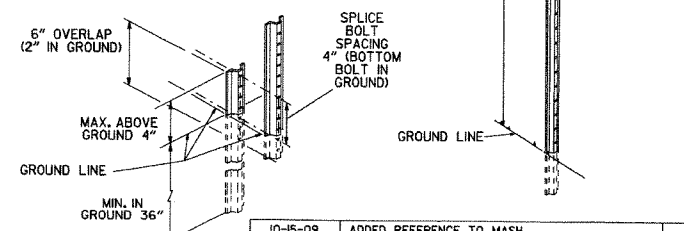


VERTICAL PANEL PLACEMENT



DETAIL OF SPLICES

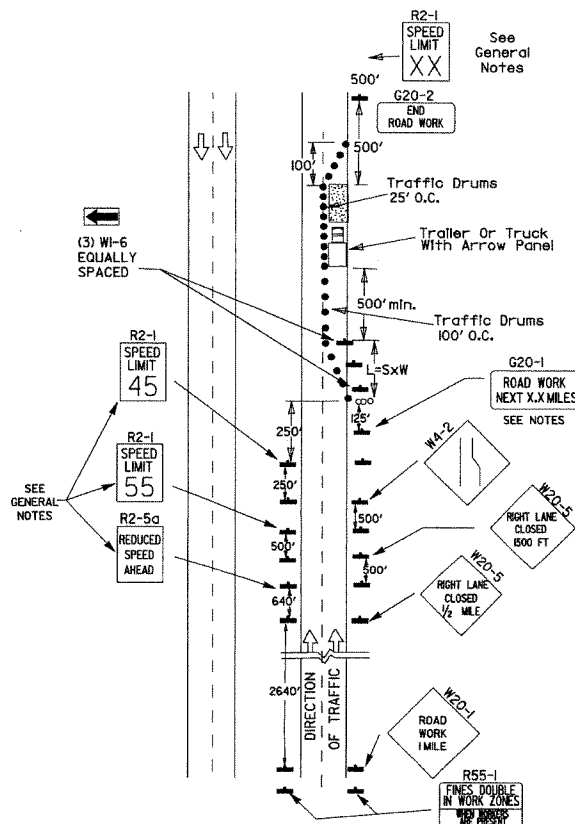
NOTE: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2) NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



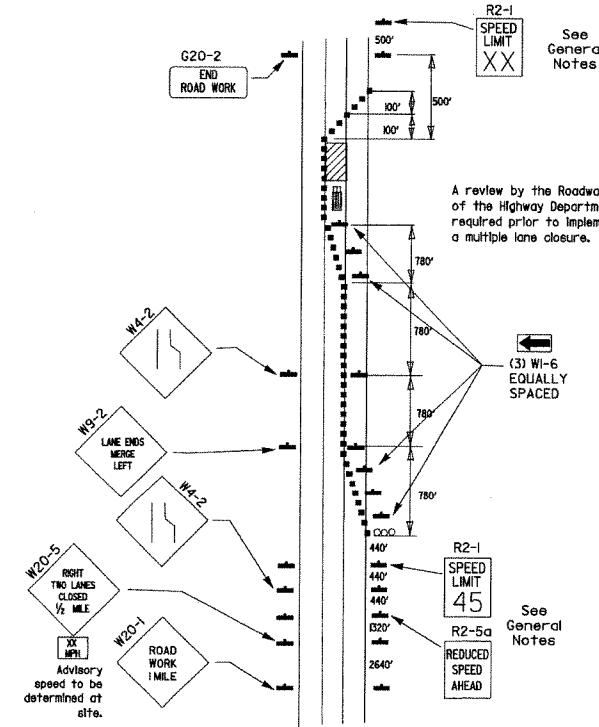
- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

GENERAL NOTES:

- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
- When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(45) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
- Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
- Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
- The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1(1/2 MILE) signs are not required in advance of lane closures that begin inside the project limits.
- Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
- All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual for Assessing Safety Hardware (MASH).
- Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.



(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

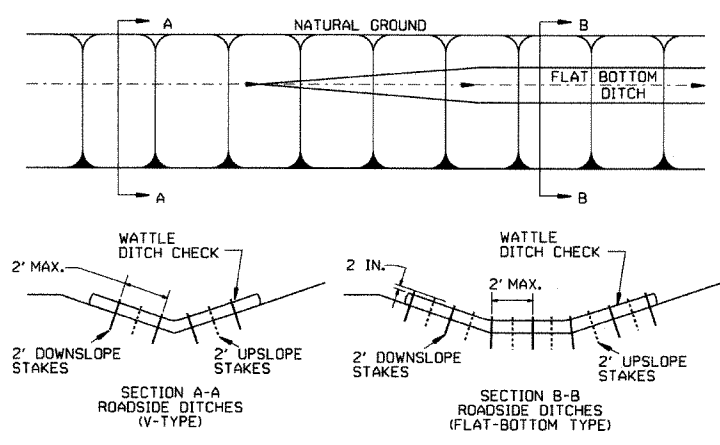


(D) Typical application - closing multiple lanes of a multi-lane highway.

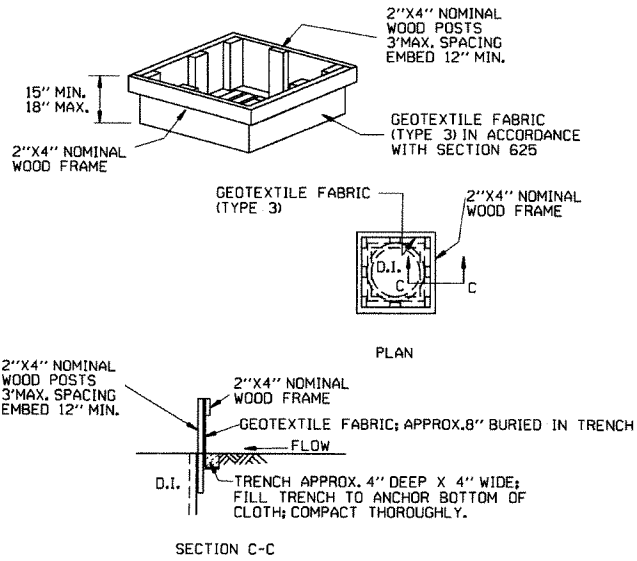
DATE	REVISION	FILMED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-3

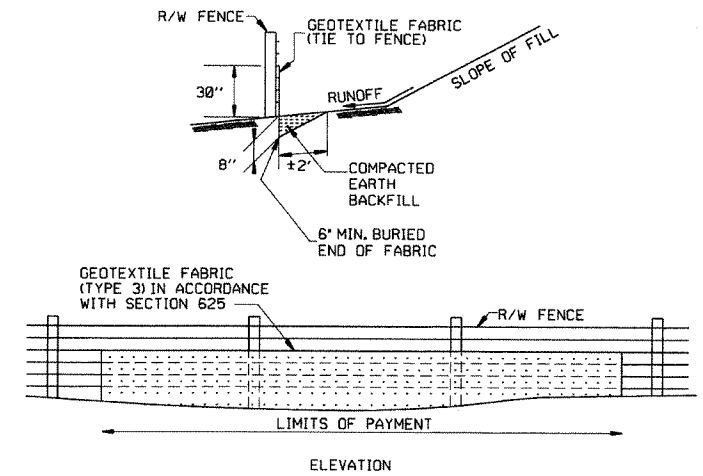
GENERAL NOTES
INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.



WATTLE DITCH CHECK (E-1)

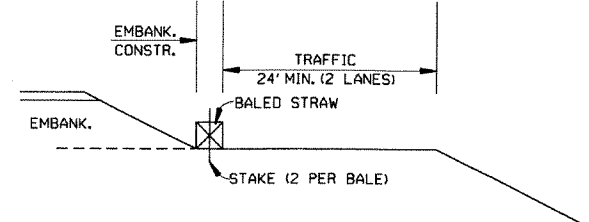


DROP INLET SILT FENCE (E-7)

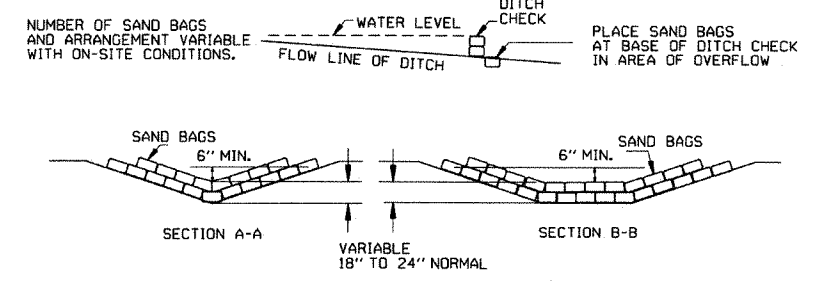


SILT FENCE ON R/W FENCE (E-4)

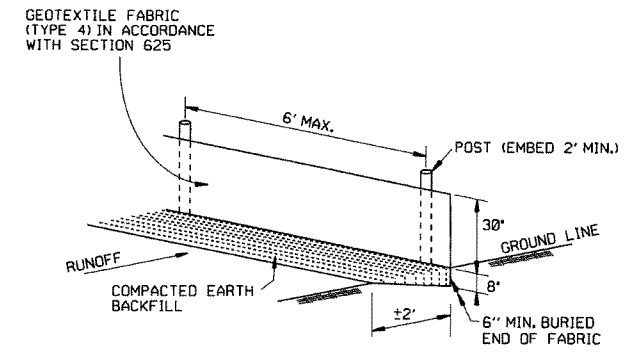
GENERAL NOTES
GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST, OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



BALED STRAW FILTER BARRIER (E-2)

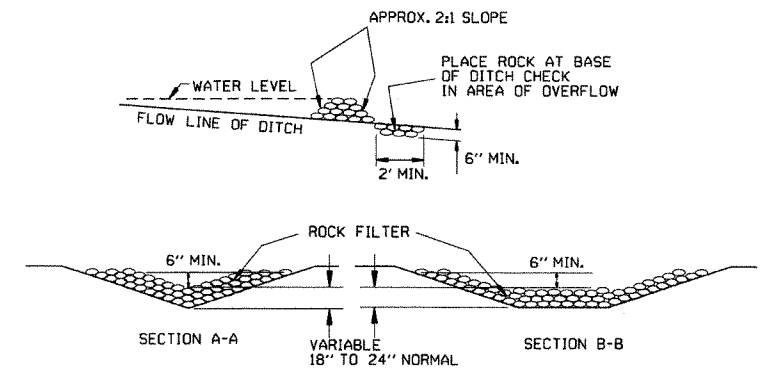


SAND BAG DITCH CHECK (E-5)



SILT FENCE (E-11)

GENERAL NOTES
GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



ROCK DITCH CHECK (E-6)

12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
7-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC	
6-2-94	REVISED E-1, 4, 7 & 11; DELETED E-2 & 3	6-2-94
4-1-93	REDRAWN	
10-1-92	REDRAWN	
8-2-76	ISSUED R.D.M.	298-7-28-76
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
TEMPORARY EROSION CONTROL DEVICES
STANDARD DRAWING TEC-1