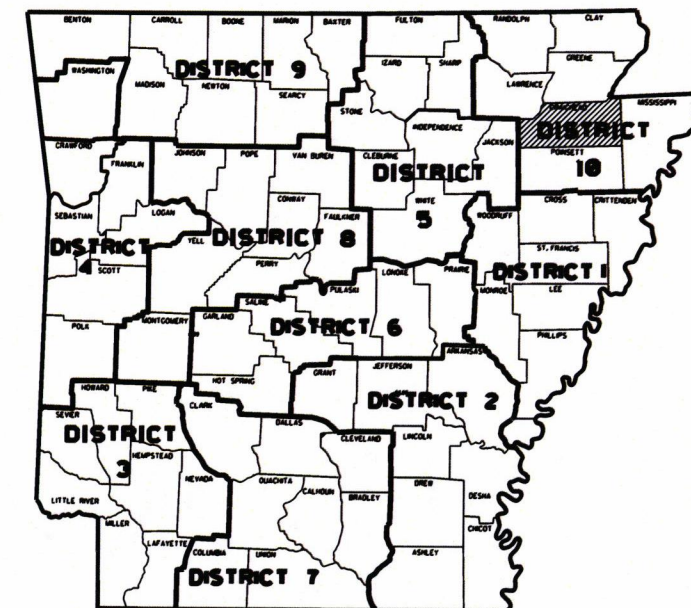


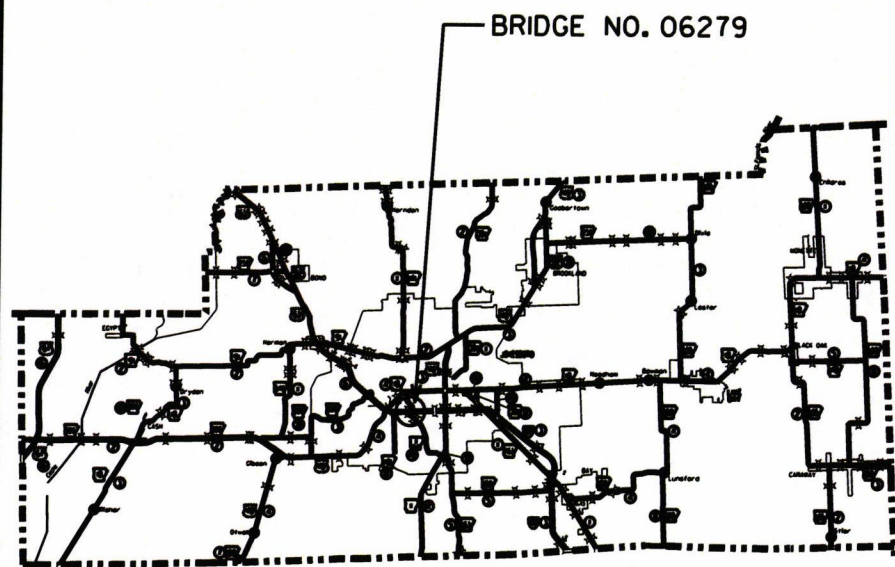
**ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT  
MAINTENANCE PLANS**

**I-555/HWY.1B EMERGENCY BRIDGE  
PAINTING (JONESBORO) (S)  
CRAIGHEAD COUNTY  
ROUTE 1B SECTION 17B  
JOB 100963**

DATE REVISION	DATE FILED	DATE REVISION	DATE FILED	FED. AID PROJ. NO.	STATE AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		
				JOB NO.	100963	1	8
① I-555/HWY 1B EMERGENCY BRIDGE PAINTING (JONESBORO) (S)							



**ARKANSAS HIGHWAY DIST. 10**

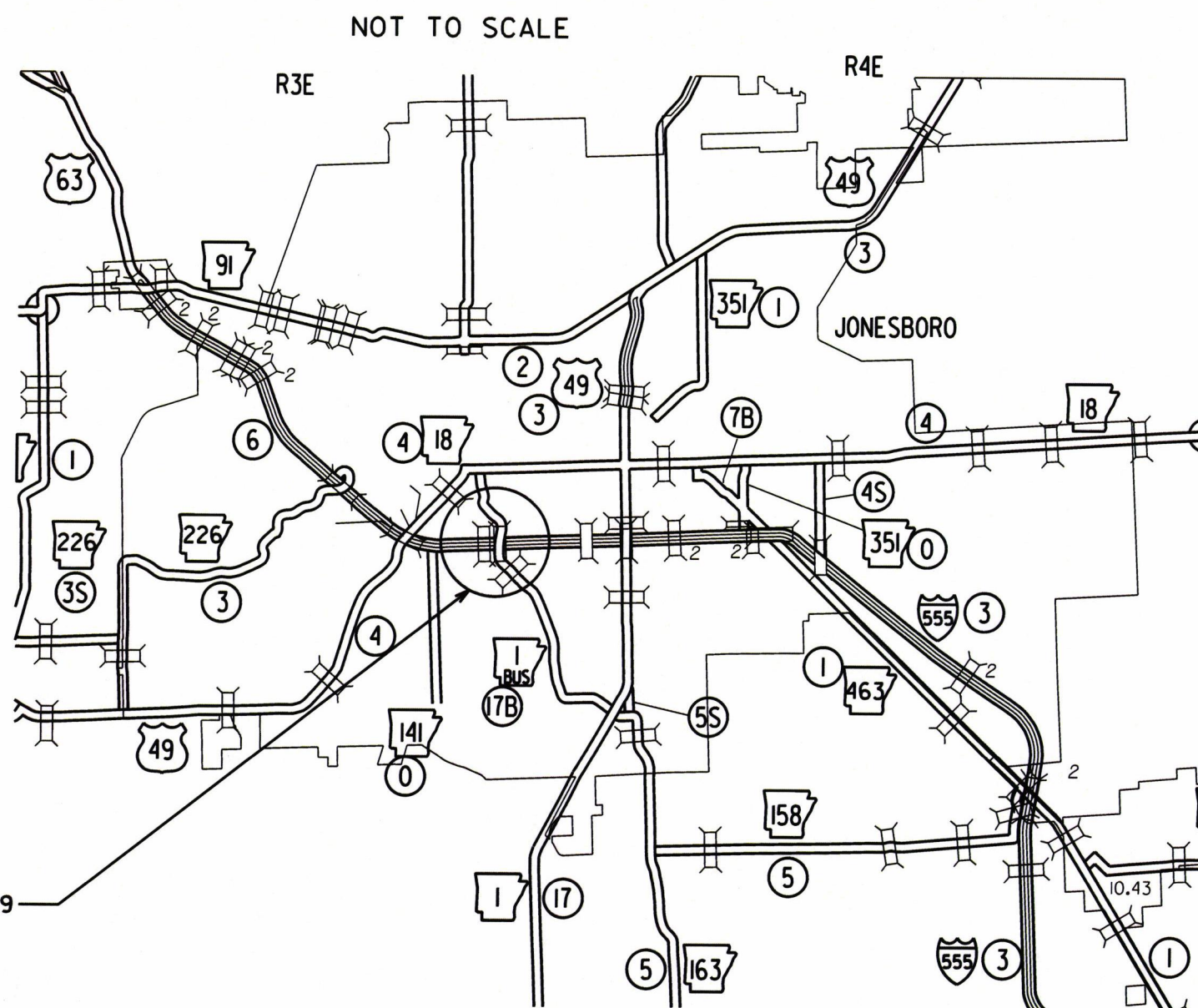


VICINITY MAP  
NOT TO SCALE

**BRIDGE DATA:**

BRIDGE NO. 06279  
HWY. 1B OVER I-555  
252'-0" CONT. COMPOSITE W-BEAM UNIT  
WITH 76'-0" CLEAR ROADWAY.

BRIDGE NO. 06279



MIDPOINT OF PROJECT: LAT. 35° 48' 26" LONG. 90° 41' 55"



DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		100963	2	8

IND. OF SHTS, GEN. NOTES, & GOV. SPECS.

### INDEX OF SHEETS

SHEET NO.

TITLE

1. TITLE SHEET
2. INDEX OF SHEETS, GENERAL NOTES, AND GOVERNING SPECIFICATIONS
3. QUANTITIES, SUMMARY OF QUANTITIES, AND REVISIONS
4. BRIDGE PICTURES
5. LAYOUT OF BRIDGE NO. 06279 - FOR INFORMATION ONLY
6. W-BEAM UNIT DETAILS- BRIDGE NO. 06279 - FOR INFORMATION ONLY
7. W-BEAM UNIT DETAILS- BRIDGE NO. 06279 - FOR INFORMATION ONLY
8. W-BEAM UNIT DETAILS- BRIDGE NO. 06279 - FOR INFORMATION ONLY

### ROADWAY STANDARD DRAWINGS

DRWG. NO.

TITLE

DATE

DRWG. NO.	TITLE	DATE
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	4-13-17
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	9-02-15
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	9-02-15

### GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER TITLE

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
100-3	CONTRACTOR'S LICENSE
102-2	ISSUANCE OF PROPOSALS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
JOB 100963	BIDDING REQUIREMENTS AND CONDITIONS
JOB 100963	CONTAINMENT SYSTEM
JOB 100963	CONTRACTOR CERTIFICATION
JOB 100963	INSPECTOR'S PERSONAL PROTECTION CLOTHING
JOB 100963	MANDATORY ELECTRONIC CONTRACT
JOB 100963	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 100963	PAINT CONTRACTOR LABEL
JOB 100963	SPECIAL MAINTENANCE OF TRAFFIC REQUIREMENTS

### GENERAL NOTES

1. TOTAL ALLOWABLE WORKING DAYS: 30 DAYS
2. PAINT SYSTEM: SEE SECTION 807 AND 820 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.  
PRIME COAT: ONE COAT OF INORGANIC ZINC, 3 MIL DFT MINIMUM UNLESS NOTED.  
INTERMEDIATE EPOXY TIE COAT: 2 MIL DFT MINIMUM  
FINISH COAT: ONE COAT URETHANE, 3 MIL DFT MINIMUM, GRAY - FEDERAL STANDARD 595B COLOR CHIP 36270  
MAXIMUM DFT FOR EACH COAT AS RECOMMENDED BY COATING MANUFACTURER.
3. ALL SURFACES TO BE PAINTED SHALL BE CLEAN AND FREE OF DUST OR OTHER OBJECTIONABLE MATTER.
4. CONTRACTOR IS RESPONSIBLE FOR BEING FAMILIAR WITH THE LOCATION OF ALL UTILITIES ON THE BRIDGES BEFORE BIDDING.
5. UTILITIES ON BRIDGES SHOULD BE PROTECTED DURING THE CLEANING AND PAINTING OPERATION.
6. CONTAINMENT REQUIRED :

BRIDGE NUMBER	CLASS OF CONTAINMENT	MIGRATORY BIRDS
06279	3	NO



INDEX OF SHEETS, GENERAL NOTES,  
AND GOVERNING SPECIFICATIONS



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				100963	3	8

**CLEANING AND PAINTING EXISTING STRUCTURAL STEEL (TYPE II)**

DESCRIPTION	QUANTITY	UNIT
BRIDGE NO. 06279	274	TON

THIS BRIDGE WAS DAMAGED BY A TRUCK ACCIDENT AND SUBSEQUENT FIRE THAT LEFT SOOT AND RESIDUE ON THE BRIDGE. STRUCTURAL REPAIRS HAVE BEEN COMPLETED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING OF THE STEEL PRIOR TO PAINTING AND DISPOSAL OF ALL WASTE IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.

**\*DISPOSAL OF HAZARDOUS WASTE**

DESCRIPTION	QUANTITY	UNIT
BRIDGE NO. 06279 (SITE NO. 1)	1.00	LUMP SUM

\* POTENTIAL HAZARDOUS WASTE IN THE FORM OF LEAD PAINT DEBRIS WILL BE REMOVED FROM THIS STRUCTURE AND SENT TO AN APPROPRIATE TREATMENT FACILITY AS PER CODE OF FEDERAL REGULATIONS 40 CFR PART 260.

**MOBILIZATION**

DESCRIPTION	QUANTITY	UNIT
ENTIRE PROJECT	1.00	LUMP SUM
TOTAL	1.00	LUMP SUM

**\*\*MAINTENANCE OF TRAFFIC**

DESCRIPTION	QUANTITY	UNIT
ENTIRE PROJECT	1.00	LUMP SUM
TOTAL	1.00	LUMP SUM

\*\* ALL TRAFFIC CONTROL DEVICES AND/OR PAVEMENT MARKINGS WILL BE PLACED IF AND WHERE APPROVED BY THE ENGINEER. ALL ITEMS NECESSARY FOR TRAFFIC CONTROL IS SUBSIDIARY TO THE ITEM OF "MAINTENANCE OF TRAFFIC".

**SUMMARY OF QUANTITIES**

LOCATION			BRIDGE DATA		ITEM NO.	SP & 820	601	SP & 603	820	
BRIDGE NUMBER	RT/SEC LOG MILE	COUNTY	ROADWAY WIDTH (FT)	ROADWAY LENGTH (FT)	BRIDGE NAME	PAY ITEM	CLEANING AND PAINTING EXISTING STRUCTURAL STEEL (TYPE II)	MOBILIZATION	MAINTENANCE OF TRAFFIC	DISPOSAL OF HAZARDOUS WASTE (SITE NO. )
						UNIT				
06279	1B/17B/2.94	CRAIGHEAD	76	252	HWY 1B OVER I-555	TON	274	LUMP SUM	LUMP SUM	LUMP SUM
TOTAL JOB NO. 100963							274	1.00	1.00	1.00 (SITE NO. 1)

**REVISIONS**

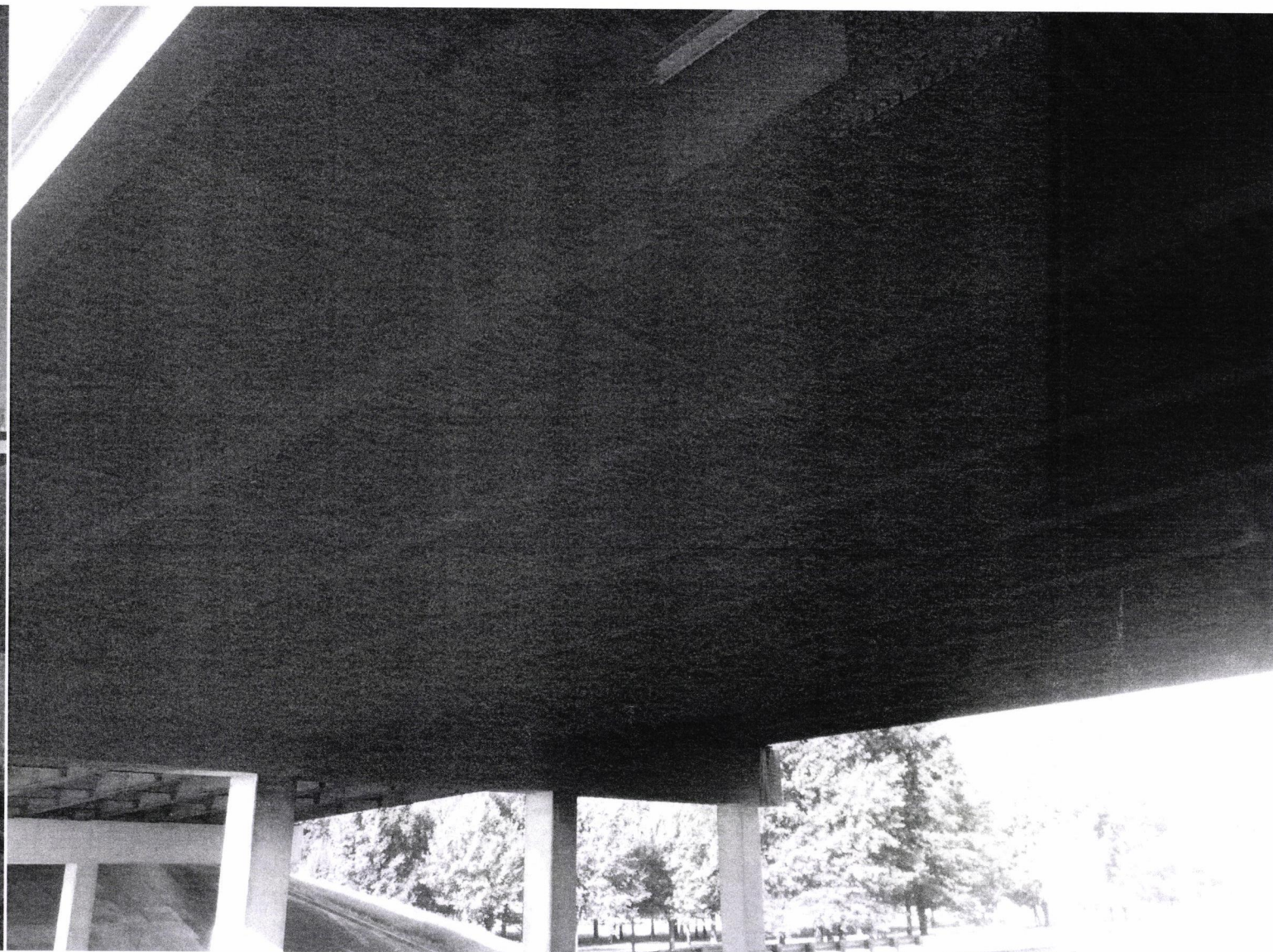
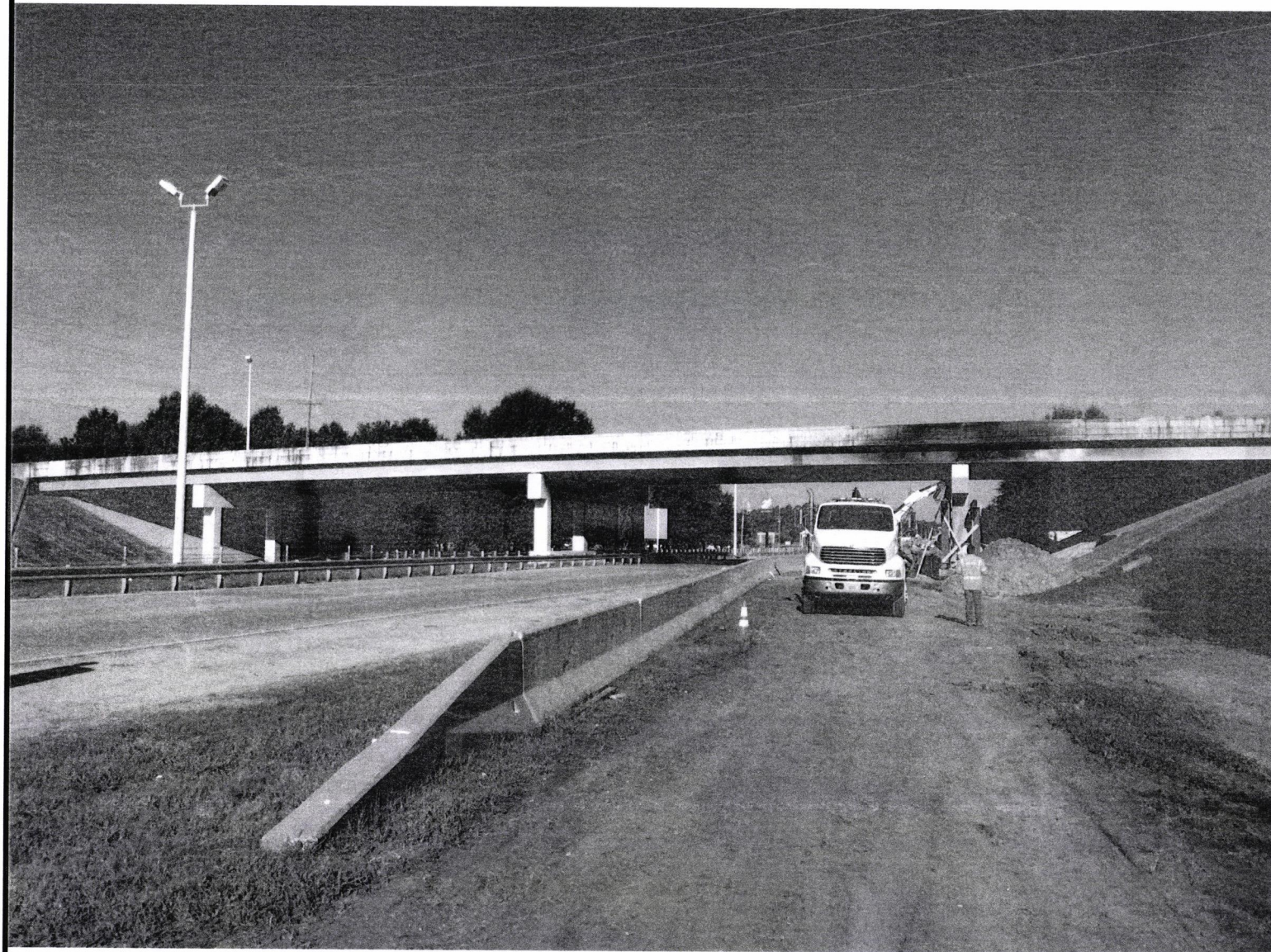
DATE	REVISION	SHEET NO.





DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		100963	4	8

BRIDGE PICTURES



BRIDGE PICTURES



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		100963	5	8

DATE REVISED	JOB NO.	SCALE	SHEET NO.	TOTAL SHEETS
	R00060	47	151	
	6279 - LAYOUT -		29353	

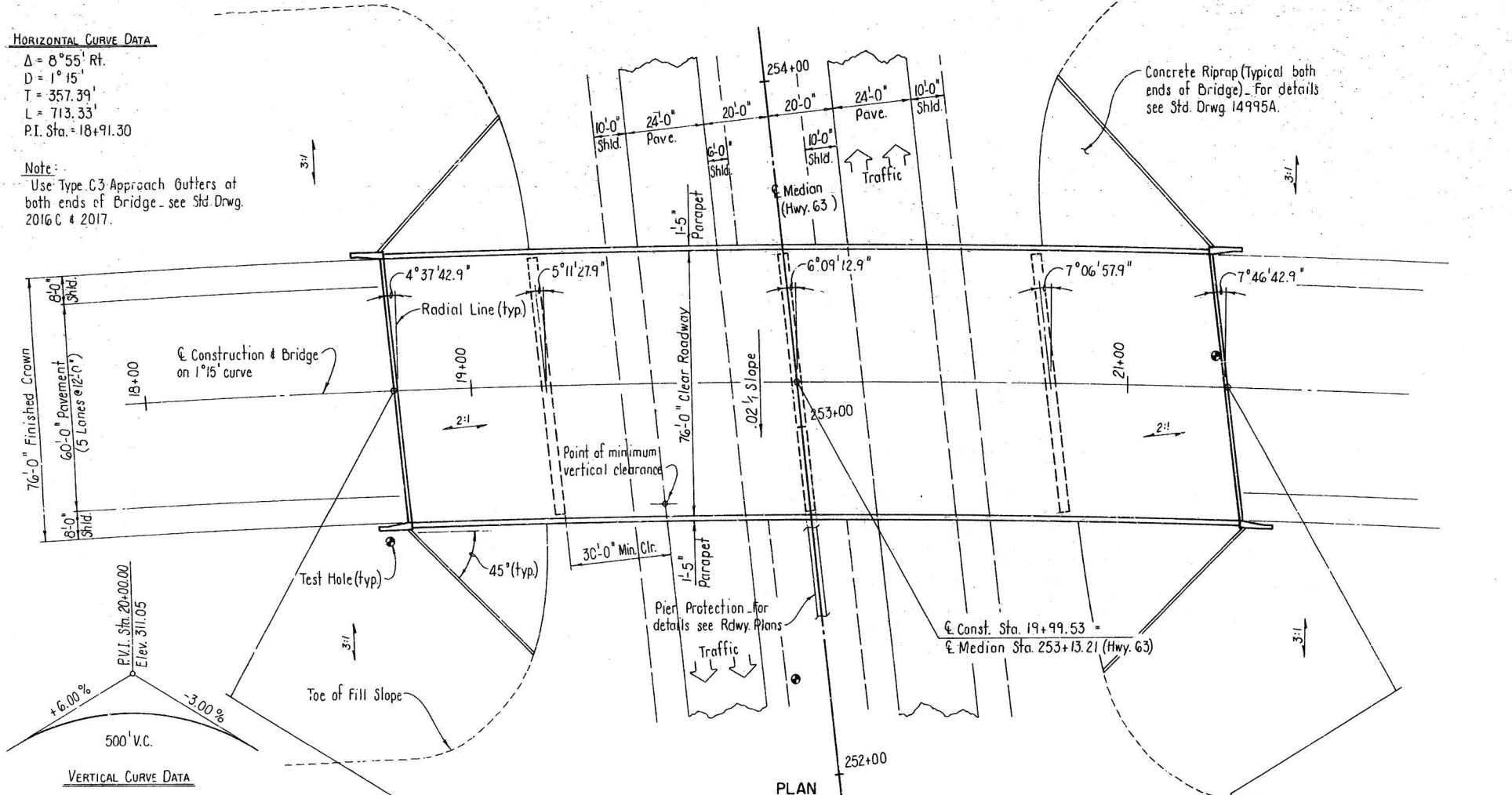
① SITE NO. 1 - FOR INFORMATION ONLY

For R/W Data, see Rdwy. Plans

**HORIZONTAL CURVE DATA**

Δ = 8°55' Rt.  
D = 1°15'  
T = 357.39'  
L = 713.33'  
P.I. Sta. = 18+91.30

Note:  
Use Type C3 Approach Outlets at both ends of Bridge. see Std. Dwg. 2016 C & 2017.



**VERTICAL CURVE DATA**

500' V.C.  
+6.00%  
-3.00%

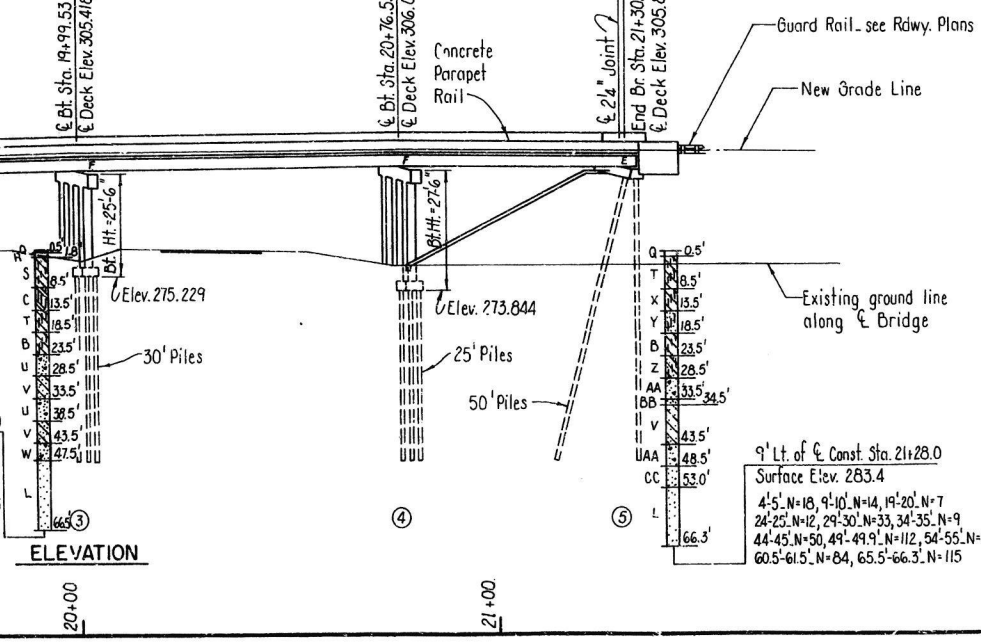
**BORING LEGEND**

- A. Moist, soft silty clay
- B. Moist, medium stiff silty clay
- C. Moist, medium dense clayey silt
- D. Moist, loose clayey silt
- E. Moist, medium stiff silty, sandy clay
- F. Moist, loose clayey sand
- G. Moist, dense silty sand
- H. Moist, dense silty sand with gravel
- J. Wet, loose silty sand
- K. Wet, soft silty, sandy clay
- L. Wet, very dense sand
- M. Wet, dense sand
- N. Wet, loose sand
- P. Wet, medium dense sand with small gravel
- Q. Asphalt pavement
- R. Concrete
- S. Moist, soft sandy, silty clay
- T. Moist, very stiff silty clay
- U. Moist, medium dense sand and gravel
- V. Moist, stiff sandy clay
- W. Moist, very dense sand and gravel
- X. Moist, stiff silty clay
- Y. Moist, medium dense silty sand
- Z. Moist, stiff sandy, silty clay
- AA. Moist, dense sand and gravel
- BB. Moist, loose sand
- CC. Moist, very dense sand

43' Rt. of C. Const. Sta. 13+73.0  
Surface Elev. 280.6  
4'-5" N=3, 9'-10" N=7, 14'-15" N=13  
19'-20" N=6, 24'-25" N=4, 29'-30" N=6  
34'-35" N=9, 39'-40" N=5, 44'-45" N=5  
49'-50" N=6, 54'-54.8" N=10, 59'-60" N=6  
64'-65" N=4, 69'-70" N=6, 74'-75" N=10  
79'-80" N=27

**PLAN**

Total Length of Bridge = 254'-2 3/8"  
252'-0" Continuous Composite W-Beam Unit  
(Spans: 45', 2 @ 77', 45')



**ELEVATION**

**GENERAL NOTES**

- BENCH MARK: NAIL IN SIDE OF POWER POLE 56' LT. OF C.L. CONST. STA. 18+90.0. SLEV. 278.48.
- DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1993 EDITION WITH CURRENT INTERIM.
- CONSTRUCTION SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1978 AND APPLICABLE SPECIAL PROVISIONS.
- DESIGN LIVE LOAD: HS20-44  
DESIGN METHOD: LOAD FACTOR
- CONCRETE: ALL CONCRETE IN SUBSTRUCTURE SHALL BE CLASS 6. ALL CONCRETE IN SUPERSTRUCTURE SHALL BE CLASS 5(AE). ALL CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH 1"2 = 3500 PSI AND SHALL BE POURED IN THE DRY, UNLESS OTHERWISE NOTED, ALL EXPOSED CORNERS SHALL BE CHAMFERED 3/4".
- DECK FINISH: THE ROADWAY SURFACE OF THE CONCRETE BRIDGE DECK SHALL BE GIVEN A TINE FINISH AS SPECIFIED FOR FINAL FINISHING IN SUBSECTION 902.23 FOR A CLASS 6, ROADWAY SURFACE FINISH.
- REINFORCING STEEL: REINFORCING STEEL SHALL CONFORM TO ASTM A615 OR A617, GRADE 60 (YIELD STRENGTH = 60,000 PSI) OR AS NOTED ON THE PLANS.
- CONCRETE SURFACES NORMALLY SPECIFIED IN SUBSECTION 902.23 TO RECEIVE "A" CLASS 2 RUBBED FINISH SHALL RECEIVE THE ALTERNATE SPRAYED FINISH INSTEAD. SEE SPECIAL PROVISION SP 902-7, "SPRAYED CONCRETE FINISH."
- ALL PILING IN BENTS 1 THRU 5 SHALL BE 16" OCTAGONAL OR 14" SQUARE PRECAST CONCRETE AND SHALL BE DRIVEN WITH AN APPROVED AIR, STEAM, OR DIESEL HAMMER TO A MINIMUM BEARING CAPACITY OF 44 TONS PER PILE AND TO A MINIMUM PENETRATION OF 20' BELOW THE GROUND LINE. DRIVE ONE 35' TEST PILE AT BENT 2 AND ONE 30' TEST PILE AT BENT 4.
- LENGTHS OF PILING SHOWN ARE ASSUMED FOR ESTIMATING PURPOSES ONLY. LENGTHS TO BE DETERMINED IN THE FIELD.
- PILES IN END BENTS TO BE DRIVEN AFTER ENLARGMENT TO BOTTOM OF CAP IS IN PLACE. PILE SHAPES SHALL NOT BE MIXED.
- FOR DETAILS OF PILING, SEE DRWG. NO. 2933
- FOR DETAILS OF APPROACH BUTTERS, SEE DRWG. NO. 2016C & 2017
- FOR DETAILS OF BENTS, SEE DRWG. NOS. 29354 - 29359
- FOR DETAILS OF 252' CONT. W-BEAM UNIT, SEE DRWG. NOS. 29360 - 29365

FOR INFORMATION ONLY

LAYOUT OF BRIDGE  
HWY. 1B OVER HWY. 63  
CRAIGHEAD COUNTY

ROUTE 1B SEC. 17B  
ARKANSAS STATE HIGHWAY COMMISSION  
LITTLE ROCK, ARK.

DRAWN BY: KMG DATE: JULY 87  
CHECKED BY: JSD DATE: AUG 87  
DESIGNED BY: JSD DATE: JUNE 87  
BRIDGE NO. 6279 DRAWING NO. 29353

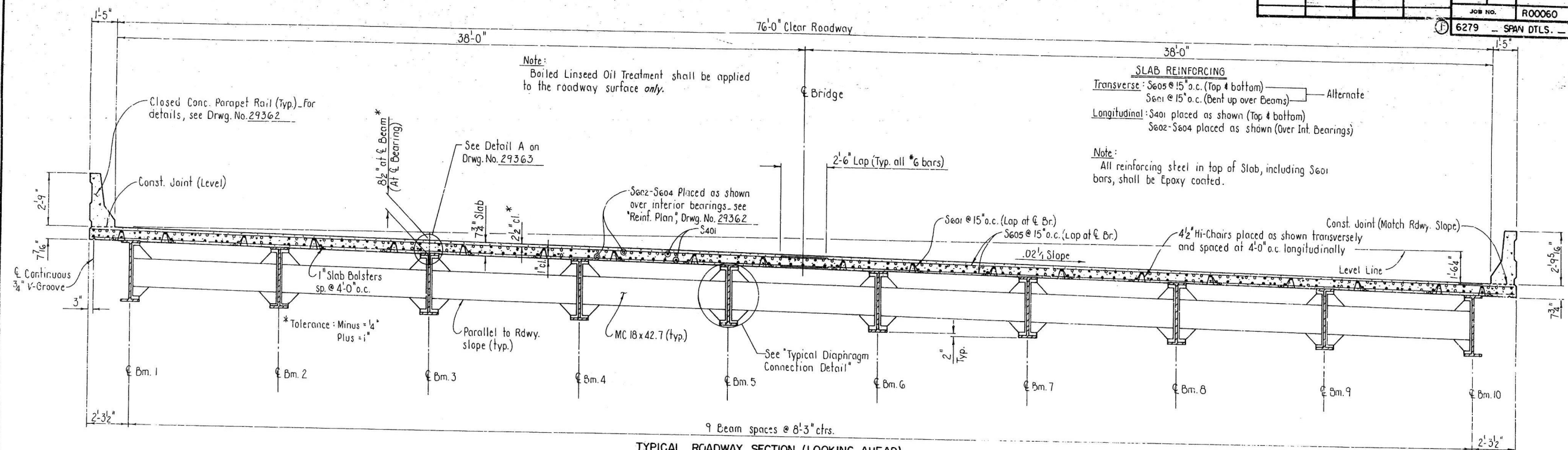
*Neal Pinkerton*  
BRIDGE ENGINEER



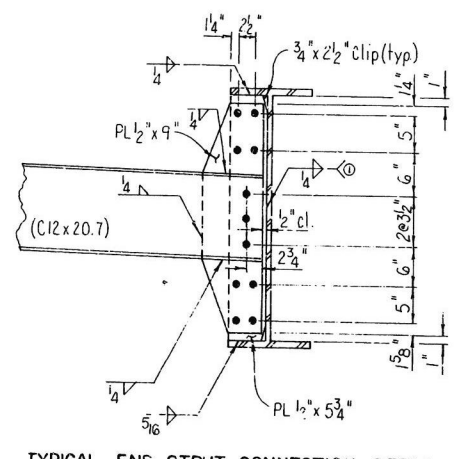
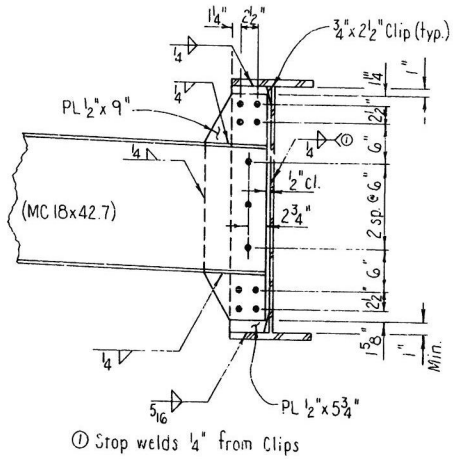
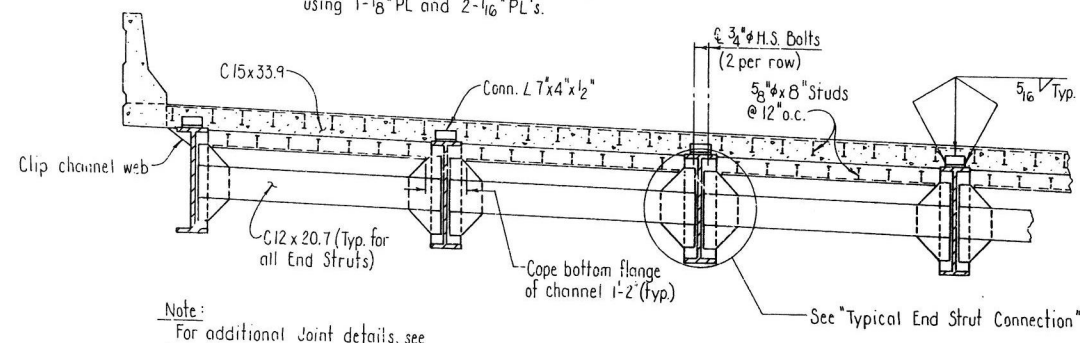
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		6	8

94  
 SITE NO. 1 - FOR INFORMATION ONLY

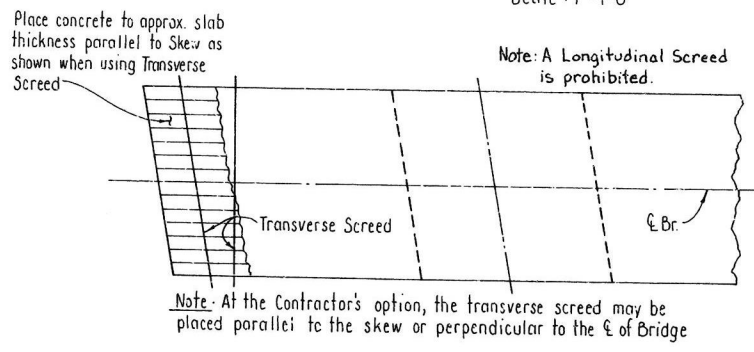
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		54	151



**EXPANSION DEVICE**  
 Roadway Channel C15x33.9  
 Connection Angle L 7"x4"x1/2"  
 5/8"x8" Stud Anchors @ 12" o.c. top & bottom  
 Preformed Joint Sealer supported by PL 1"x3/4"  
 Note:  
 Detail device 1/8" high and provide 1/4" Shims using 1-1/8" PL and 2-1/16" PL's.



For "General Notes", see Drwg. No. 29365



**FOR INFORMATION ONLY**

SHEET 1 OF 4  
 DETAILS FOR  
 252'-0" CONTINUOUS COMPOSITE  
 W-BEAM UNIT  
 CRAIGHEAD COUNTY  
 ROUTE 18 SEC. 17B

ARKANSAS STATE HIGHWAY COMMISSION  
 LITTLE ROCK, ARK.

DRAWN BY: KMG DATE: AUG 87  
 CHECKED BY: CRH DATE: 9-2-87  
 DESIGNED BY: USJ DATE: Nov 87

*General*  
 BRIDGE ENGINEER

BRIDGE NO. 6279 DRAWING NO. 29360



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		7	8

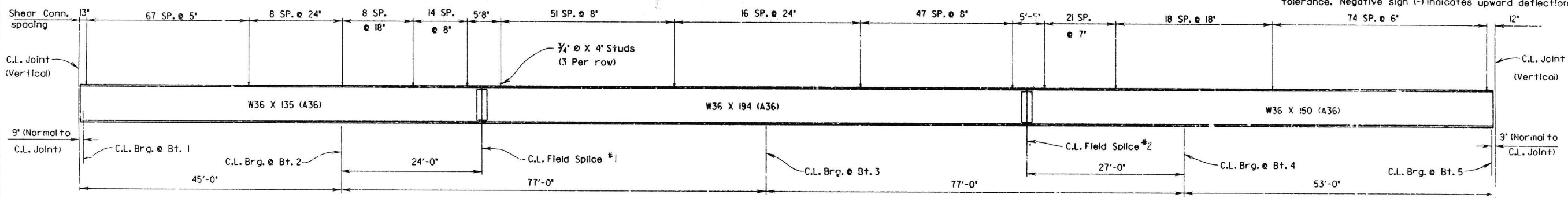
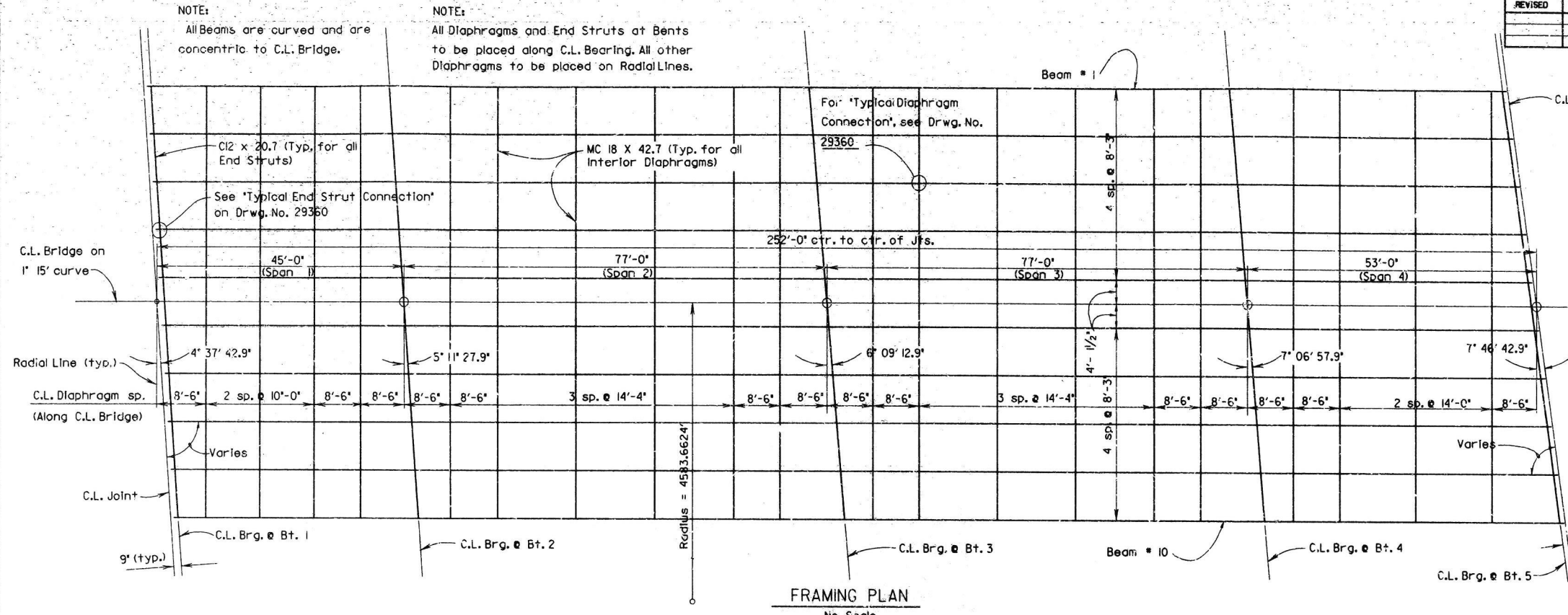
JOB NO. 100963 SITE NO. 1 - FOR INFORMATION ONLY

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	JOB NO.	SCALE
				6279 - SPAN DTLS. - 29361	55 151

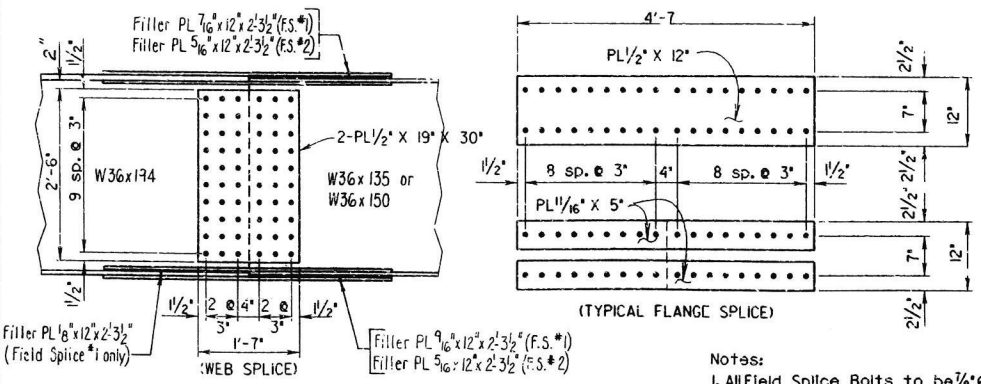
TABLE OF DEFLECTIONS (INCHES)

Point of Defl.	Structural Steel		Str. Steel (+) Slab		Str. Steel (+) Slab (+) Rail	
	Int. Bm.	Ext. Bm.	Int. Bm.	Ext. Bm.	Int. Bm.	Ext. Bm.
1.0	0.000	0.000	0.000	0.000	0.000	0.000
1.1	0.003	0.003	0.030	0.023	0.031	0.027
1.2	0.006	0.005	0.052	0.042	0.054	0.048
1.3	0.006	0.005	0.063	0.049	0.065	0.057
1.4	0.005	0.004	0.064	0.045	0.066	0.055
1.5	0.002	0.000	0.061	0.047	0.063	0.055
1.6	-0.002	-0.003	0.045	0.033	0.046	0.039
1.7	-0.006	-0.007	0.019	0.003	0.020	-0.009
1.8	-0.008	-0.009	-0.008	-0.008	-0.009	-0.009
1.9	-0.008	-0.008	-0.032	-0.026	-0.033	-0.030
2.0	0.000	0.000	0.000	0.000	0.000	0.000
2.1	0.032	0.030	0.053	0.025	0.058	0.045
2.2	0.073	0.070	0.055	0.290	0.366	0.336
2.3	0.10	0.102	0.522	0.425	0.538	0.493
2.4	0.11	0.120	0.642	0.499	0.632	0.580
2.5	0.128	0.122	0.624	0.508	0.644	0.591
2.6	0.113	0.108	0.553	0.450	0.571	0.524
2.7	0.085	0.081	0.414	0.337	0.428	0.393
2.8	0.050	0.048	0.24	0.196	0.248	0.228
2.9	0.016	0.015	0.079	0.064	0.082	0.076
3.0	0.000	0.000	0.000	0.000	0.000	0.000
3.1	0.012	0.011	0.056	0.046	0.058	0.054
3.2	0.040	0.038	0.165	0.159	0.202	0.187
3.3	0.072	0.069	0.350	0.286	0.362	0.335
3.4	0.096	0.093	0.474	0.387	0.490	0.451
3.5	0.108	0.105	0.535	0.437	0.553	0.50
3.6	0.105	0.102	0.522	0.426	0.539	0.496
3.7	0.088	0.085	0.433	0.353	0.448	0.412
3.8	0.057	0.055	0.280	0.229	0.290	0.267
3.9	0.022	0.022	0.111	0.090	0.115	0.106
4.0	0.000	0.000	0.000	0.000	0.000	0.000
4.1	-0.002	-0.003	-0.005	-0.004	-0.005	-0.004
4.2	0.003	0.002	0.033	0.025	0.034	0.029
4.3	0.013	0.011	0.089	0.070	0.092	0.081
4.4	0.023	0.020	0.145	0.115	0.143	0.132
4.5	0.030	0.027	0.185	0.146	0.190	0.169
4.6	0.034	0.030	0.20	0.160	0.207	0.184
4.7	0.032	0.029	0.188	0.150	0.193	0.173
4.8	0.025	0.023	0.145	0.117	0.150	0.134
4.9	0.014	0.012	0.080	0.064	0.082	0.07
5.0	0.000	0.000	0.000	0.000	0.000	0.000

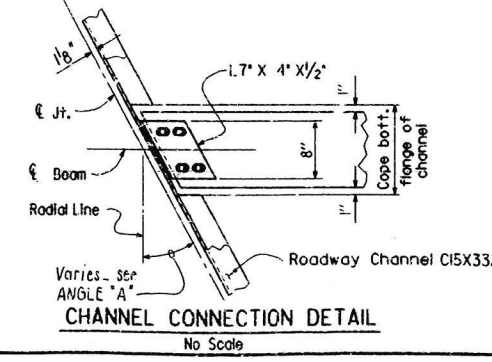
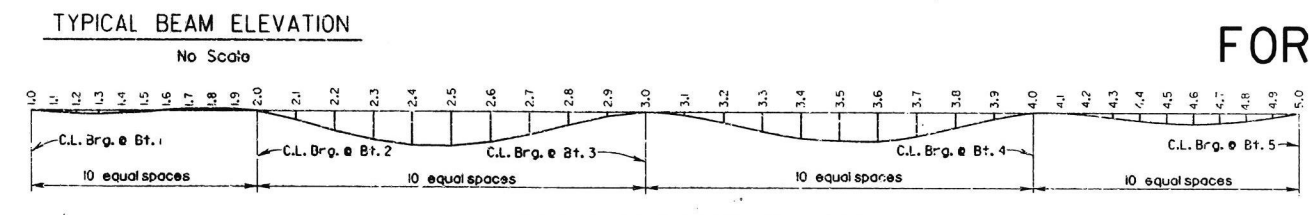
Note: Camber for Deflection plus vertical curve  $\pm 1/4"$  tolerance. Negative sign (-) indicates upward deflection.



Note: For details of Elastomeric Bearings, see Drwg. No. 29364



- Notes:  
 1. All Field Splice Bolts to be  $3/8"$  H.S. Bolts  
 2. All Field Splice Plates to be A36 Steel.



ANGLE "A"

Beam No.	Bt. 1	Bt. 5
1	4°35'28.7"	7°42'56.5"
2	4°35'58.4"	7°43'46.5"
3	4°36'28.1"	7°44'36.7"
4	4°36'57.9"	7°45'27.0"
5	4°37'27.9"	7°46'17.6"
6	4°37'57.9"	7°47'08.3"
7	4°38'28.1"	7°47'59.2"
8	4°38'58.4"	7°48'50.3"
9	4°39'28.8"	7°49'41.5"
10	4°39'59.3"	7°50'33.0"

*Wesley Pinkerton*  
 BRIDGE ENGINEER

SHEET 2 OF 4  
 DETAILS FOR  
 252'-0" CONTINUOUS COMPOSITE  
 W-BEAM UNIT  
 CRAIGHEAD COUNTY  
 ROUTE 1B SEC. 17 B  
 ARKANSAS STATE HIGHWAY COMMISSION  
 LITTLE ROCK, ARK.  
 DRAWN BY: KMG DATE: AUG 87  
 CHECKED BY: CRH DATE: 9-8-87 SCALE: AS SHOWN  
 DESIGNED BY: LBB DATE: JAN 87  
 BRIDGE NO. 6279 DRAWING NO. 29361

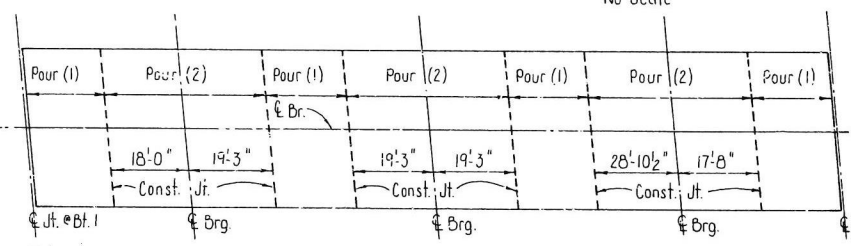
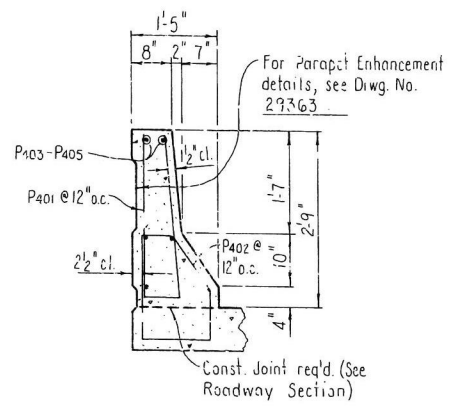
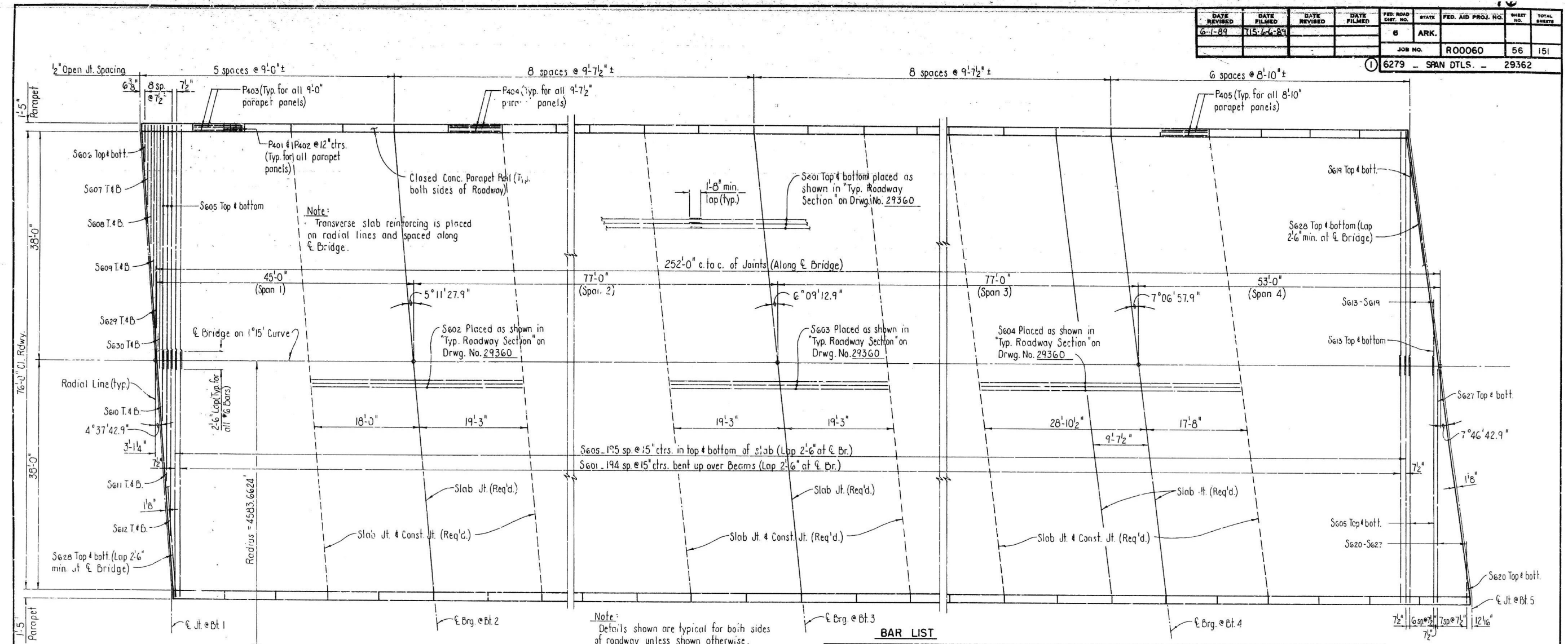


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		100963	8	8

SITE NO. 1 - FOR INFORMATION ONLY

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
6-1-89	7-15-89			6	ARK.			
				JOB NO.		R00060	56	151

6279 - SPAN DTLS. - 29362

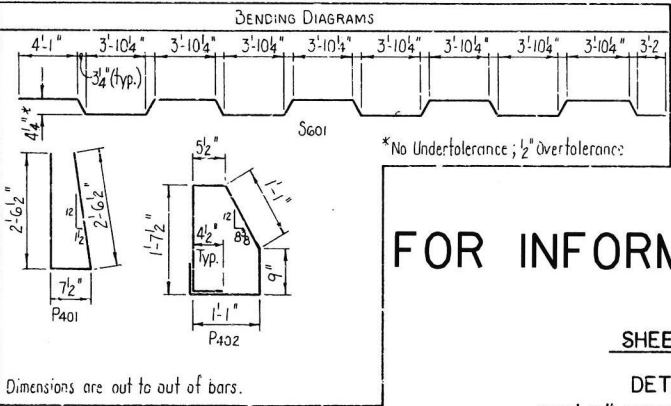


Note:  
 Pours with the same number may be poured simultaneously or separately. All Pours (1) must be placed before Pours (2) can be placed. 48 hours shall elapse between pours and 72 hours shall elapse between adjacent pours. Any Railing pours made before the entire slab unit has been placed must be approved by the Bridge Engineer.

Note:  
 See "Concrete Placement Procedure", Drwg. No. 29360

**BAR LIST**


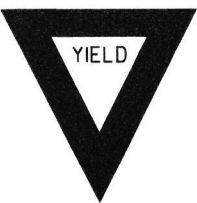




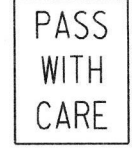


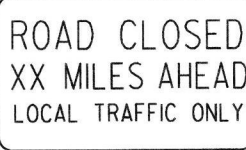
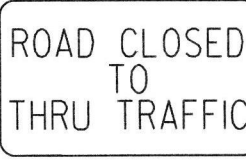

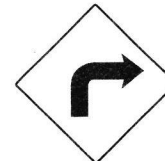



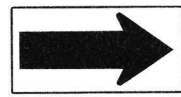

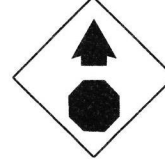
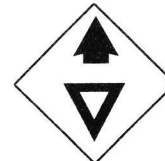
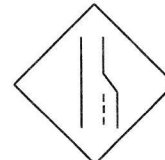













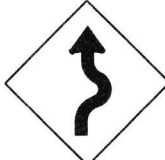




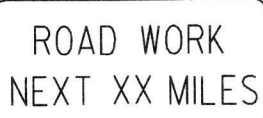
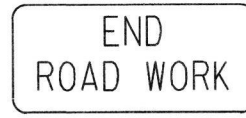
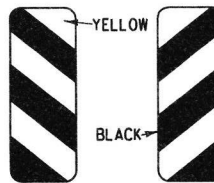


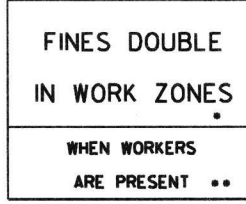
MARK	No. Req'd.		LENGTH	PIN DIA.
	REGULAR	EPOXY COATED		
S401	735	406	37'-6"	Str.
S601	-	390	41'-6"	4 1/2"
S602	20	96	37'-3"	Str.
S603	20	96	38'-6"	Str.
S604	20	96	46'-6"	Str.
S605	402	402	40'-4"	Str.
S606-S609	1 of each	1 of each	Varies 5'-9" to 28'-11"	Str.
S610-S612	1 of each	1 of each	Var. 14'-4" to 29'-10"	Str.
S613-S619	1 of each	1 of each	Var. 6'-0" to 33'-5"	Str.
S620-S627	1 of each	1 of each	Var. 7'-5" to 39'-5"	Str.
S628	4	4	40'-11"	Str.
P401	518	-	5'-6"	2"
P402	518	-	5'-4"	2"
P403	60	-	8'-8"	Str.
P404	192	-	4'-4"	Str.
P405	72	-	8'-6"	Str.
S629	1	1	36'-8"	Str.
S630	1	1	44'-4"	Str.



**FOR INFORMATION ONLY**

SHEET 3 OF 4  
 DETAILS FOR  
 252'-0" CONTINUOUS COMPOSITE  
 W-BEAM UNIT  
 CRAIGHEAD COUNTY  
 ROUTE 1B SEC. 17B  
 ARKANSAS STATE HIGHWAY COMMISSION  
 LITTLE ROCK, ARK.  
 DRAWN BY: KMG DATE: AUG 87  
 CHECKED BY: CRH DATE: 9-8-87  
 DESIGNED BY: JBB DATE: JULY 87  
 SCALE: AS SHOWN  
 BRIDGE NO. 6279 DRAWING NO. 29362



<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

ADVANCE DISTANCES  
(XXXX)

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE
	AHEAD

GENERAL NOTES:

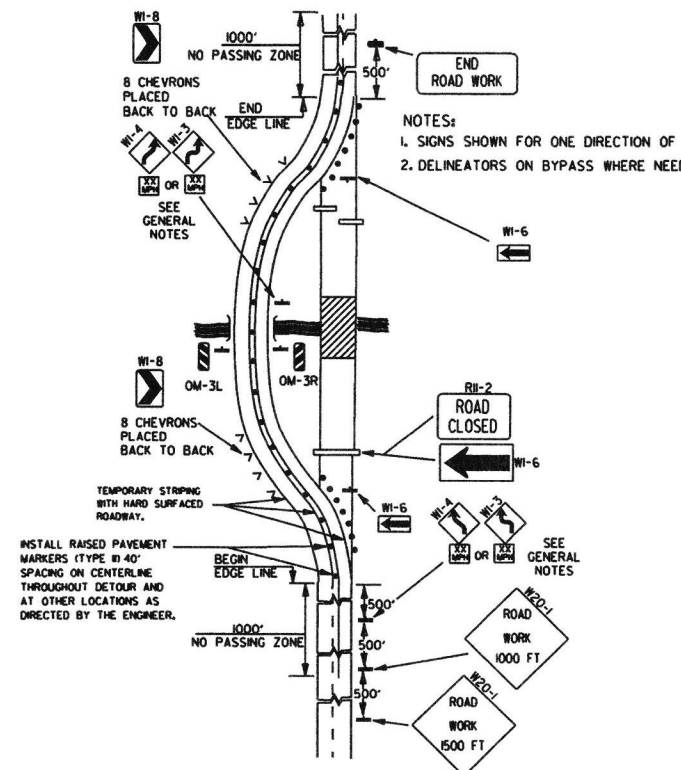
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACE, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

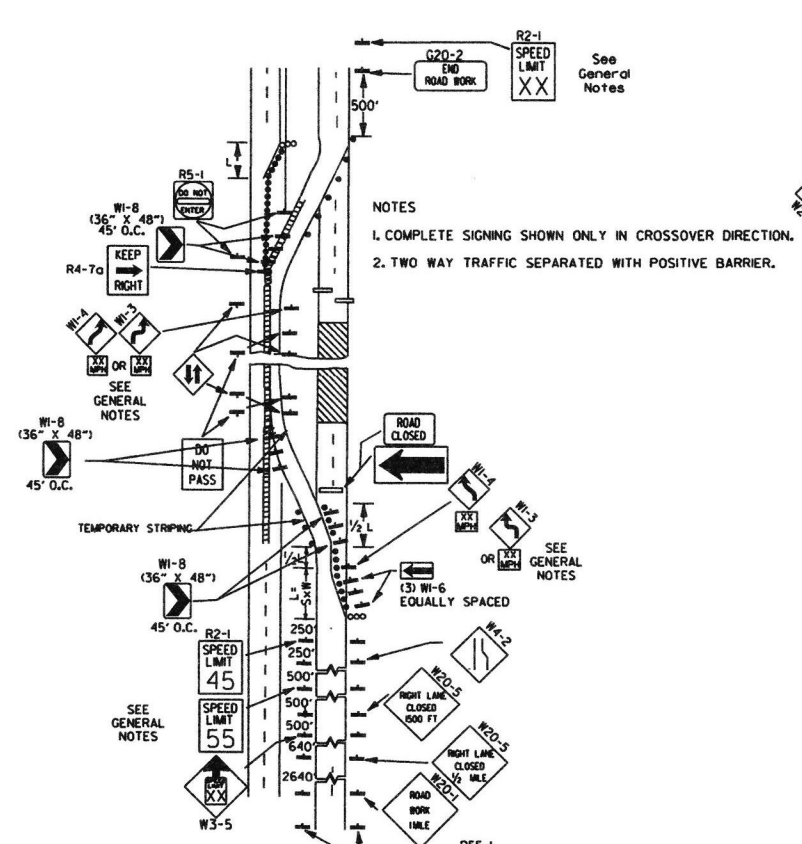
4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILED

ARKANSAS STATE HIGHWAY COMMISSION  
STANDARD TRAFFIC CONTROLS  
FOR HIGHWAY CONSTRUCTION  
STANDARD DRAWING TC-1

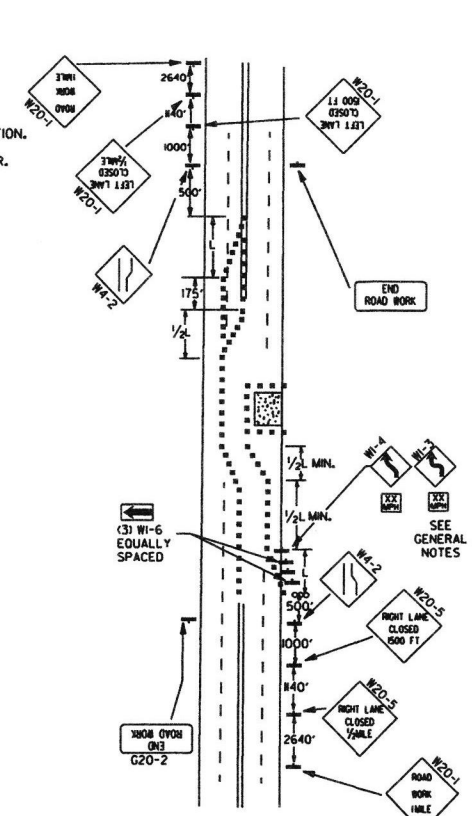




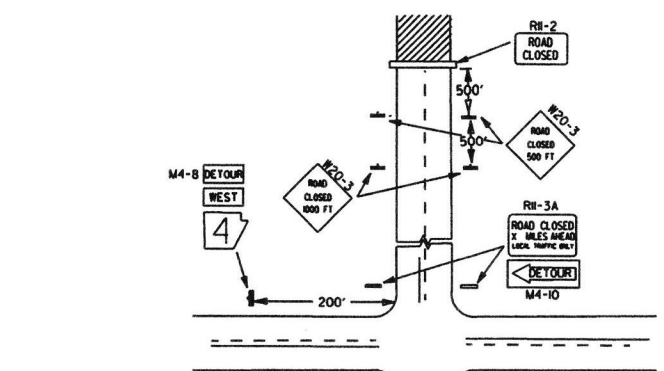
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



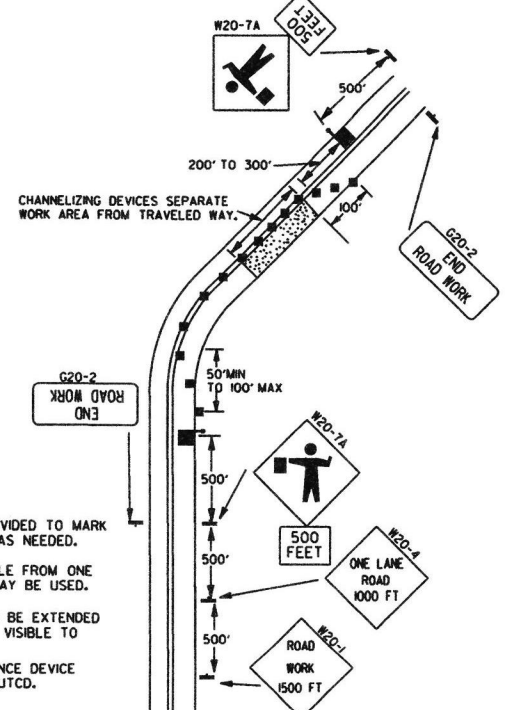
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



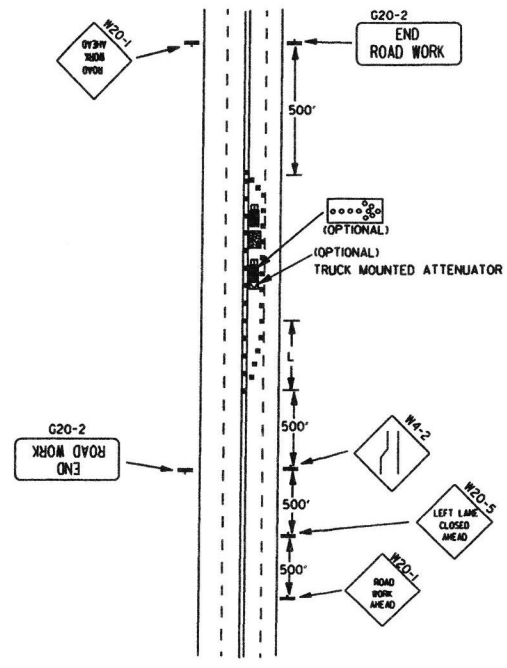
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



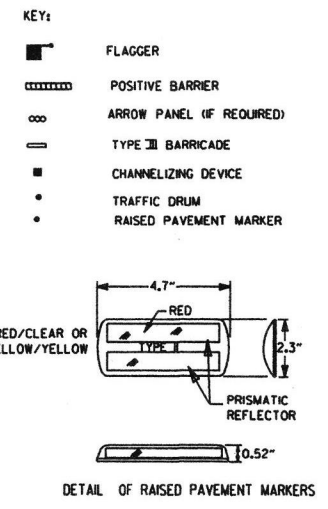
(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.



TYPICAL ADVANCE WARNING SIGN PLACEMENT

TAPER FORMULAE:  
 $L = SW$  FOR SPEEDS OF 45MPH OR MORE.  
 $L = \frac{WS^2}{60}$  FOR SPEEDS OF 40MPH OR LESS.  
 WHERE:  
 L = MINIMUM LENGTH OF TAPER.  
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.  
 W = WIDTH OF OFFSET.

- GENERAL NOTES:  
 1. ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.  
 2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-155 SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1XXI SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.  
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-145 SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1XXI SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.  
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.  
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.  
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.  
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.  
 8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

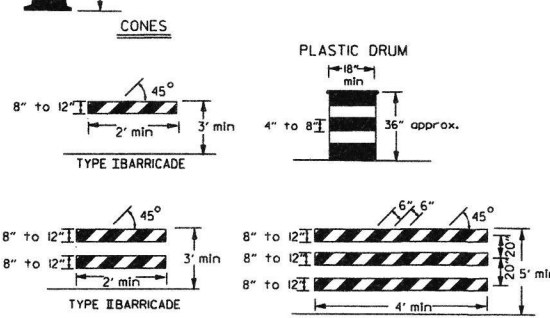
DATE	REVISION	FILED
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-8-10	ADDED (AFAD)	
8-20-08	REVISED SIGN DESIGNATIONS	
8-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION  
 STANDARD TRAFFIC CONTROLS  
 FOR HIGHWAY CONSTRUCTION  
 STANDARD DRAWING TC-2



Channelizing devices

• When cones are used on freeways and multi-lane highways, they shall be 28" min. During hours of darkness, 28" cones shall be used on all roadways, and shall be reflectorized in accordance with the M.U.T.C.D.



NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.

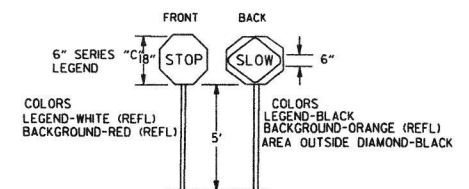
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	WB-11
1" to 3"	Edge of shoulder	WB-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-land vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

\* When shown on the plans concrete barrier will be used. When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.

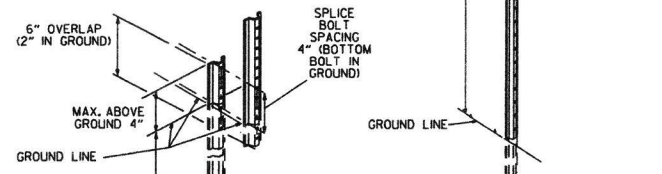


STOP SLOW PADDLE



DETAIL OF SPLICES

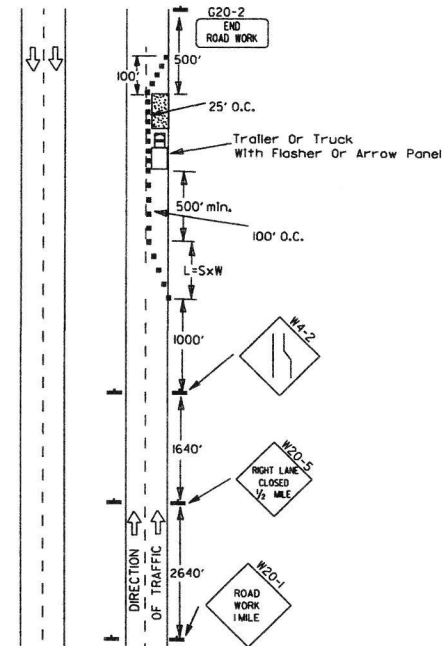
NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2) NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



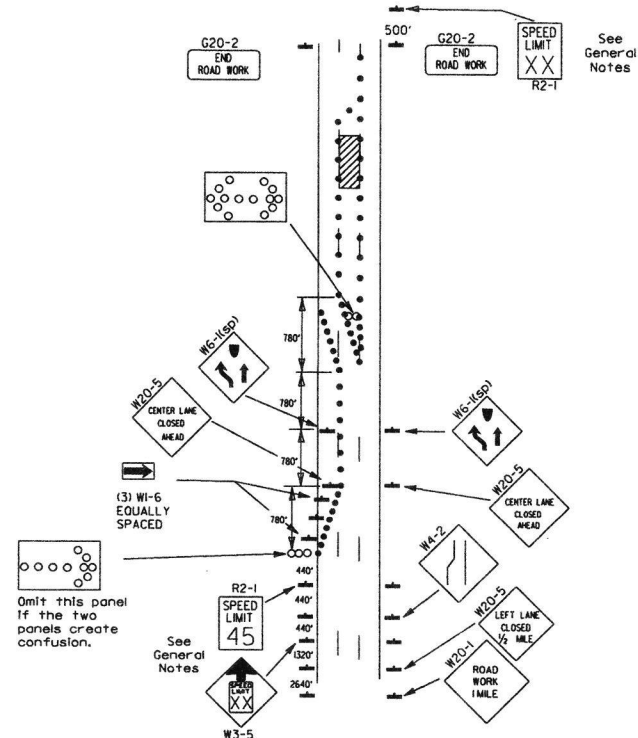
DATE	REVISION	FILMED
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5	
10-15-09	ADDED REFERENCE TO MASH	
8-20-08	REVISED SIGN DESIGNATIONS	
8-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION  
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION

STANDARD DRAWING TC-3



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

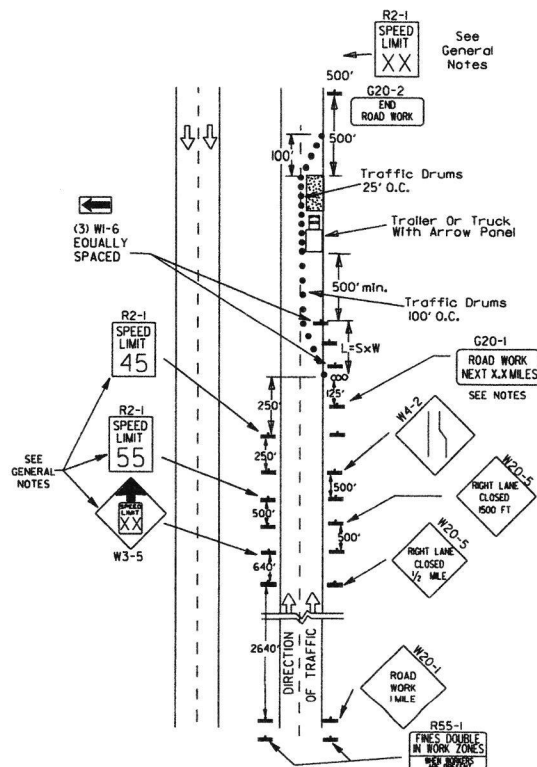


(B) Typical application - 3-lane oneway roadway where center lane is closed.

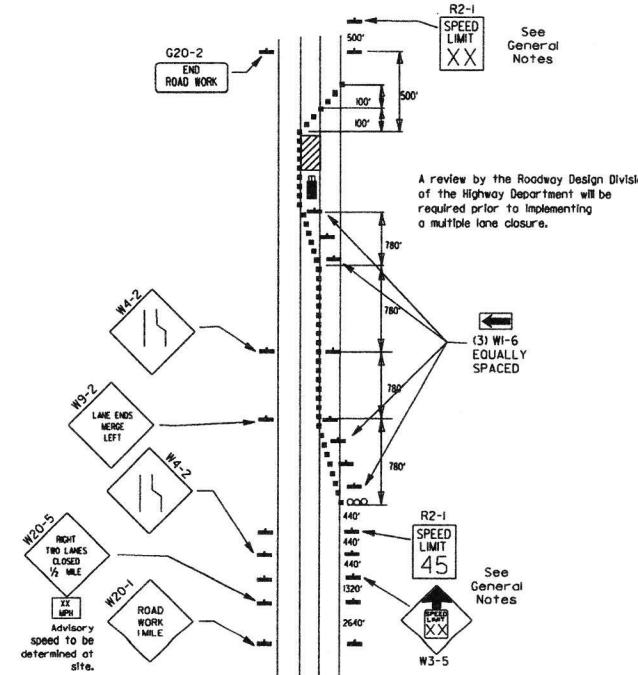
- KEY:
- Arrow Panel (if Required)
  - Channelizing Device
  - Traffic drum

GENERAL NOTES:

- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
- When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the W3-5 shall be installed at that location. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-1(55) speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
- Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
- Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
- The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1(1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
- Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
- All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
- Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.



(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.



(D) Typical application - closing multiple lanes of a multilane highway.